



# The Oasis at Indio Specific Plan

ADMINISTRATIVE DRAFT

June 2024



# The Oasis at Indio Specific Plan

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# Chapter 1 - Introduction

## 1.1 Planning Context

The Oasis at Indio Specific Plan establishes the land use and development regulations, and implementation strategies to provide a balanced and sustainable residential and non-residential development with a range of employment-generating activities and medium to high-density residential that will complement surrounding neighborhoods, centers, and districts. The intent of this Specific Plan is to provide a comprehensive framework in which development can occur in a planned, logical fashion rather than a piecemeal approach. This comprehensive approach will help to create a unified development for the City of Indio residents and visitors to enjoy.

The Oasis at Indio Specific Plan is a stand-alone planning document that focuses on the unique characteristics of the Specific Plan Area and customizes the planning standards and review procedures to reflect the City’s vision for this area within the Avenue 42 Corridor Subarea. Additionally, The Oasis at Indio Specific Plan addresses infrastructure, public services, and utilities to ensure comprehensive and coordinated planning of the essential facilities needed to support the land uses described in this plan. The Specific Plan is the tool that will systematically implement the goals and policies of the City of Indio General Plan. Proposed development plans or agreements, tentative tract or parcel maps, and any other development approval must be in substantial conformance with the Specific Plan. Projects which are found consistent with the Specific Plan will be deemed consistent with the City's General Plan. Upon adoption by ordinance, The Oasis at Indio Specific Plan shall serve as the official zoning and development plan for the Specific Plan Area.

### 1.1.1 Existing Setting and Surrounding Uses

The Oasis at Indio is located within the Coachella Valley area of Riverside County, in the north-central portion of the City of Indio, adjacent to and north of Interstate 10 (I-10) as shown in Figure 1-1, *Regional Map* and Figure 1-2, *Vicinity Map*. The Specific Plan Area is bounded by Avenue 42 to the north, Monroe Street to the east, the Thousand Palms Stormwater Channel to the west, and the I-10 Freeway to the south.

Regional access to the Specific Plan Area is available from the I-10 Freeway via Monroe Street. Primary arterial access to the Specific Plan Area is available from Monroe Street, Jackson Street, and Jefferson Street, which are accessible from/to I-10.

The Specific Plan Area consists of seven Assessor’s Parcel Numbers (APNs): 610-020-001, 610-020-010, 610-020-012, 610-020-013, 610-020-021, 610-020-034, 610-020-036. While the APNs total 173.9 acres, the property has been surveyed reflecting a gross acreage of approximately 182.8 acres.

Figure 1-1: Regional Map

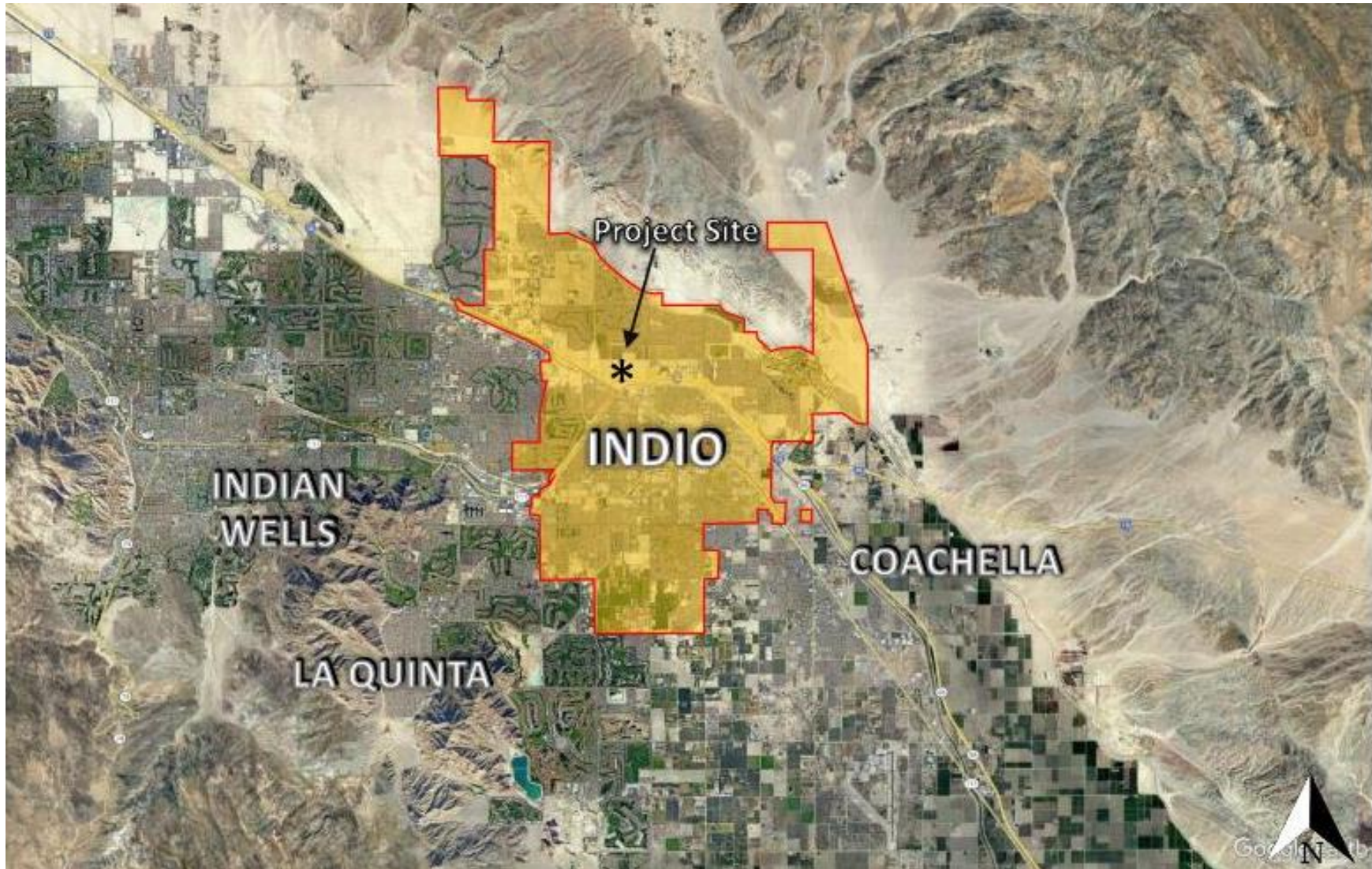


Figure 1-2: Vicinity Map



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The overall site is relatively flat with a slight gradient from northwest to southeast. There is approximately 15 feet of fall across the site with an average slope of about 0.3%. The Specific Plan Area is currently undeveloped vacant land with a few remnants of former development/structures for agricultural purposes including several concrete pads on the north-central portion of the subject property, and two agricultural standpipes on the northwest corner and central portion of the property.

The Site is not located within a 100-year or 500-year flood zone, there are no sole-source aquifers, groundwater monitoring wells, and no evidence of public water sources within the Specific Plan Area.

The site is surrounded by active agriculture land uses and vacant land to the north, the Thousand Palms Stormwater Channel, vacant land, and a landscaping supply facility (Southwest Boulder & Stone) to the west, commercial land uses (The Palms at Indio) to the east, and the I-10 Freeway to the south. The southeast corner of Monroe Street and Avenue 42 includes a Circle K convenience store/gas station, drive-thru restaurants, financial business, urgent care facility, and other miscellaneous commercial/retail uses. North of the Specific Plan beyond the agricultural land uses, are several residential neighborhoods including Palazzo and Sun City Shadow Hills. South of the Specific Plan beyond the I-10 Freeway includes the Coachella Valley Stormwater Channel as shown in Figure 1-4, *Existing Site Conditions and Surrounding Uses*.



Figure 1-3: Existing Site Conditions and Surrounding Uses



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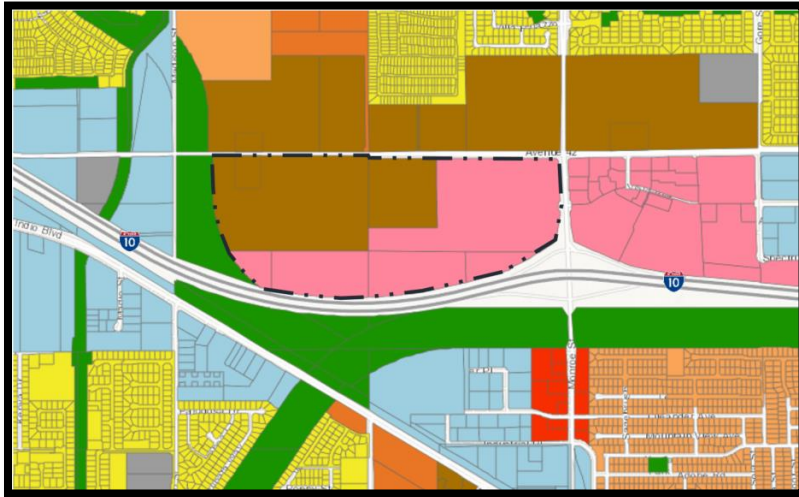
### 1.1.2 Existing and Proposed General Plan Land Use and Zoning

#### 1.1.2.1 General Plan

The City of Indio 2040 General Plan is a long-range policy document that provides implementation measures to achieve the City’s long-term vision to enhance the local economy, maintain the “City of Festivals” reputation, and simultaneously improve access to quality education, housing, and jobs.

The 2040 General Plan sets forth land use designations called place types that indicate the purpose and intended use for each parcel within the City. The place types specify additional guidance related to development form and character including allowable uses, building density, building placement, parking location and access, building frontage, and streetscape design. The existing General Plan designation within the Specific Plan Area is Mixed-Use Neighborhood (MUN) within the northwesterly area and Regional Commercial (RC) within the northeasterly and southerly area as shown in Figure 1-5, *Existing and Proposed General Plan Place Type*.

Figure 1-4: Existing General Plan Place Type Designations



Proposed General Plan Place Type Designations



## Chapter 1 Introduction

Mixed-Use Neighborhood supports moderate- to higher-intensity neighborhood development that features a variety of multifamily housing choices and commercial uses along major streets. Allowed land uses include a range of single-family and multi-family residential uses, parks and recreation, mixed-use (retail/office, retail/residential, office/residential), and neighborhood-serving retail, general commercial, services, entertainment, live/work, and office.

Regional Commercial supports large-format retail development along with associated restaurant and commercial service activity. Allowed land uses include large-format retail, commercial services, lodging, entertainment, restaurants, parks and recreation.

Table 1-1 General Plan Place Type Designation		
Place Type and Intent	Max. Gross Density/FAR	Zoning Correlation
Mixed Use Neighborhood (MUN)	DU/AC: Up to 40 FAR: up to 0.5	RH, NC, CC, P
Regional Commercial (RC)	FAR: 0.35 to 1.0	RC, CC

\*This results in a maximum allowable residential dwelling units of 3,240 and commercial square footage of 3,318,357-6,204,686.

In addition to Place Type Designations, the General Plan includes key strategies for specific subareas to encourage infill development ensuring the result is a connected, complete place with housing and commercial uses organized into a walkable, transit-supportive pattern of neighborhoods and centers. The Oasis at Indio Specific Plan is located within Indio's subarea, Avenue 42 Corridor. Avenue 42 Corridor is comprised of large areas of undeveloped land on both sides with good freeway access and is close to many of Indio's newest neighborhoods.

While Avenue 42 Corridor encourages a pattern of connected neighborhoods, centers, and employment districts to leverage the economic and fiscal value of that area for future generations, the ultimate composition of the area may vary in response to market conditions.

With the adoption of The Oasis at Indio Specific Plan, the General Plan Land Use (Place Types) will be reclassified from MUN and RC to Specific Plan (SP) to establish an appropriate land use and zoning district designation that optimizes the Specific Plan Area's potential and facilitates achievement of the Specific Plan's goals and objectives.

This refinement is consistent with the community's desire to provide more specific guidance on the intended future character of the City and allows the City to establish zoning designations that similarly communicate form and character.

### 1.1.2.2 Zoning

The City Council adopted new citywide zoning regulations called the Unified Development Code that became effective on October 22, 2022. The existing zoning designation for the site is Specific Plan (MU-

SP), Mixed-Use Neighborhood (MUN), and Regional Commercial (RC). With the adoption of The Oasis at Indio Specific Plan, the Specific Plan Area will be reclassified and zoned Specific Plan (SP) to permit the implementation of the proposed project subject to management and implementation tools to ensure that the goals and policies are fully satisfied in compliance with the City’s General Plan.

Table 1-2 Zoning Designation	
Zoning	Density/FAR
Regional Commercial	1.0
Mixed Use Neighborhood	20-40 du/ac; 0.50
Gateway SP (MU 7)	20 du/ac; 0.25

\* This results in a maximum allowable residential dwelling units of 929-1,338 and commercial square footage of 4,375,602.

Figure 1-5: Existing Zoning



Proposed Zoning



### 1.1.3 Relationship with the Bermuda Dunes Airport Plan

The Bermuda Dunes Airport is a privately-owned, public-use general aviation airport located along the central, western edge of Indio. As a result of the airport, certain restrictions apply to new projects within its Airport Influence Area as identified in the adopted Riverside County Airport Land Use Compatibility Plan (ALUCP) for Bermuda Dunes Airport. These zones and the land use restrictions associated with them are outlined in the 2004 ALUCP and included in Appendix A of this Specific Plan.

The Oasis at Indio Specific Plan is located within the influence area for the Bermuda Dunes Airport and Airport Compatibility Zones C, D, and E overlay the property.

As required by State law, the adoption of a specific plan affecting the property within an airport influence area (Public Utilities Code Section 21676(b)) and the adoption or approval of a zoning ordinance or building regulation which (1) affects property within an airport influence area, and (2) involves the types of airport impact concerns listed in Section 1.4 (Public Utilities Code Section 21676(b)) shall be referred to the Airport Land Use Commission (ALUC) for determination of consistency with the Commission’s Plan prior to the approval by the City of Indio.

The Riverside County Airport Land Use Commission held a public hearing on February 8, 2024, and issued a Finding of Consistency for the project on February 13, 2024. The ALUC approval letter, conditions of approval, and Airport Notice are included in Appendix A of this Specific Plan. ALUC Conditions of approval have been fully incorporated into this Specific Plan as identified in Appendix A.

### 1.1.4 Previous Entitlements

Several Master Plan entitlements were approved within the Specific Plan Area. In 1995, the Gateway Conceptual Specific Plan (GCSP) was approved, covering over 2,700 acres and proposing a planned mixture of commercial, business, office, and residential uses in Indio. The western portion of the project site (approximately 80 acres west of the Clinton Street alignment) is located within the GCSP, designated as Mixed-Use Planning Area MU7. To implement the GCSP, the City required that each planning area have approval of one or more Project Master Plans at a more detailed level of site design.

In 2007, the City of Indio approved the “Thymes Square” Project Master Plan on 63.2 acres (APN: 610-020-010) located on the southeast corner of Avenue 42 and the Thousand Palms flood control channel right-of-way. The approval authorizes the development of Mixed Use on the Gateway Conceptual Specific Plan that includes 217 Residential units (32.6 acres) on the western half of the master plan, Open Space/Recreation (5-acre public park), Commercial/Business Park development (10 acres) on the northeastern portion, and Light Industrial/Business Park development (11.1 acres) on the southeastern portion of the master plan site.

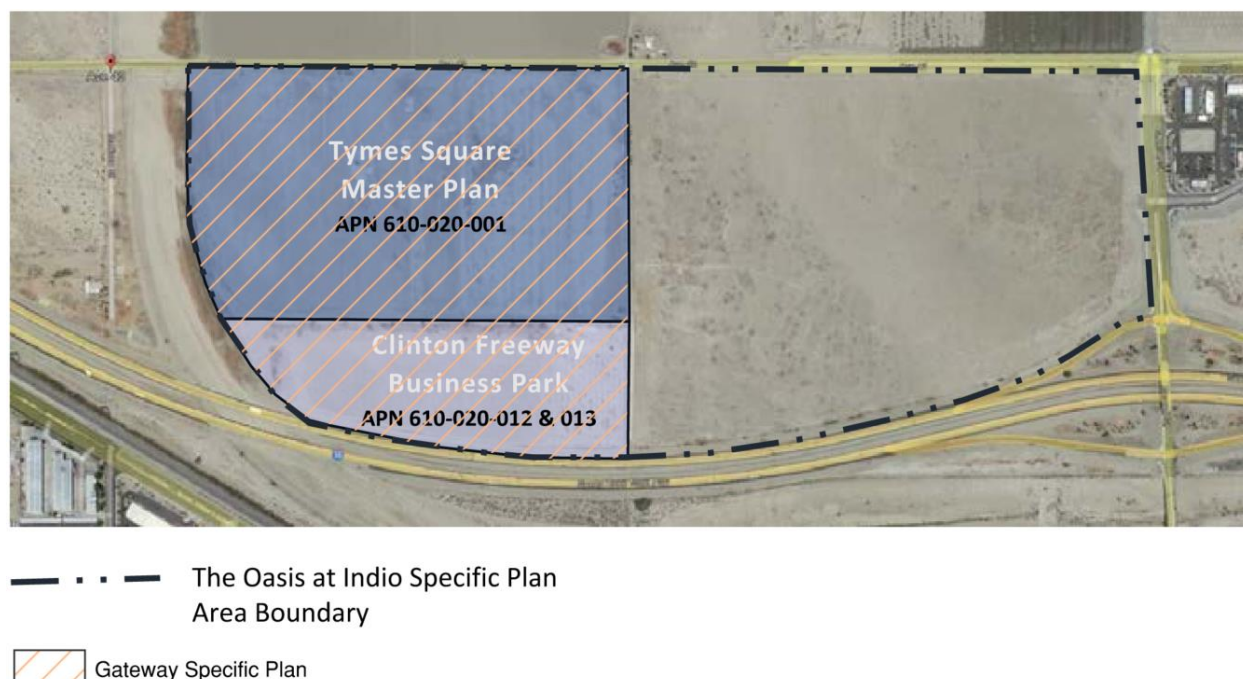
In 2008, the City of Indio approved the Clinton Freeway Business Park Project Master Plan - A 22.6-acre project (APN: 610-020-012 and 610-020-013) immediately south of the “Thymes Square” mixed-use project. The approval authorizes the development of a Mixed-Use business park with three basic components: showroom retail businesses (6.3 acres) taking advantage of the site’s direct freeway visibility, light industrial businesses (11 acres) requiring large indoor spaces for light manufacturing,

## Chapter 1 Introduction

assembly, and storage as well as “industrial incubator” units (4.3 acres) westerly of Clinton Street with more modest square footages to serve small businesses.

Neither Project Master Plan has been implemented and both sites currently remain vacant. The 80-acre portion located within the Gateway Specific Plan shall be withdrawn prior to the adoption of The Oasis at Indio Specific Plan.

### Figure 1-6: Previous Entitlements



## 1.2 Authority and Requirements

A “Specific Plan” is a planning and regulatory tool made available to local governments by the State of California. Specific plans implement an agency’s General Plan through the development of policies, programs, and regulations that provide an intermediate level of detail between General Plans and individual development projects. State law stipulates that specific plans can only be adopted or amended if they are consistent with an adopted General Plan.

The authority to prepare, adopt and implement specific plans is provided by California Government Code Sections 65450 through 65457. Procedures for preparing, processing, reviewing, adopting, and amending specific plans are provided in Article 6, Subsection 6.05.03, Chapter 22.46 of the City of Indio Unified Development Code. The State allows Specific Plans, including any associated regulations and programs, to be adopted by resolution or ordinance; approval of this specific plan shall be by ordinance in compliance with the City of Indio Unified Development Code. As required by the City of Indio and the California Government Code, the Specific Plan includes text and diagrams which specify the following in detail:

- The distribution, location, and extent of land uses proposed within the area covered by the Specific Plan, including open space areas.
- The proposed distribution, extent, intensity, and location of major components of public and private circulation/transportation, drainage, energy, sewers, solid waste disposal, water, and other essential facilities proposed to be located within the Specific Plan Area and needed to support the proposed land uses.
- Standards, criteria, and guidelines by which development would proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.
- A program of implementation measures, including financing, regulations, programs, and public works projects, necessary to carry out the proposed land uses, infrastructure, and development and conservation standards and criteria.
- A discussion of the relationship of the Specific Plan to the actions, goals, objectives, and policies of the adopted General Plan inclusive of detailed analysis as to how the Specific Plan complies with the actions, goals, objectives, and policies.
- Additional information deemed to be necessary by the Director based on the characteristics of the area to be covered by the plan, applicable actions, goals, objectives, and policies of the General Plan, or any other issue(s) determined by the Director to be significant.

For legislative changes, including Specific Plans, Specific Plan Amendments, General Plan Amendments, Development Code, and Zoning Map Amendments, the Planning Commission shall review the requested action and make a recommendation to the City Council, with the Council serving as the final decision-making body. Both the Planning Commission, in its advisory role, and the City Council, in its approval role, may approve, deny, or modify a specific plan.

The Oasis at Indio Specific Plan is the regulatory document guiding land uses and development within the boundaries identified in this Specific Plan. Upon adoption by ordinance by Council, this Specific Plan will provide the zoning design and development standards and regulations, as well as infrastructure systems, on which subsequent project-related development activities are to be based. Concurrently with adopting the Specific Plan, the General Plan 2040 shall be amended by resolution of the Council to specifically refer to The Oasis at Indio Specific Plan and change the land use designation of the area encompassed in the Specific Plan Area from Mixed-use Neighborhood (MUN) and Regional Commercial (RC) to Specific Plan (SP) on the General Plan Land Use Map. Additionally, the Zoning Map shall be amended and approved by the City Council to change the zoning of Specific Plan (MU-SP), Mixed-Use Neighborhood (MUN), and Regional Commercial (RC) to Specific Plan (SP) on the Zoning Map.

### 1.3 CEQA Compliance

An Environmental Impact Report (EIR) has been prepared for The Oasis at Indio Specific Plan in accordance with the California Environmental Quality Act (CEQA) and the implementing State CEQA Guidelines (California Code of Regulations, Title 14, Section 15000 et seq.). The EIR evaluates potential environmental impacts based on the highest development intensity associated with the two land use plan scenarios with Maximum Buildout Scenario (MBS) reflecting the most intensive development.

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- MBS: A mixed-use development of up to 3,240 multi-family residential units and 20,000 sf of commercial use on approximately 75.1 acres within Planning Areas (PA) 1, 2, and 3; and 1,806,290 sf of light Industrial use on approximately 92.8 acres within PA 4.
- Scenario #2: A mixed-use development of up to 1,237 multi-family residential units and 71,600 square feet of commercial use including a 128 key hotel on approximately 75.1 acres within PAs 1, 2, and 3; and 1,806,290 sf of Industrial use on approximately 92.8 acres within PA 4.

The EIR also identifies feasible mitigation measures and potentially feasible alternatives to address any significant and adverse impacts of the Specific Plan on the environment. The City of Indio is required, by CEQA, to certify the EIR prior to the approval of The Oasis at Indio Specific Plan.

Later activities that have been adequately analyzed under the EIR and are consistent with the specific plan are exempt from the requirements of CEQA, as long as subsequent projects are (1) within the scope of the approved EIR, and (2) the EIR adequately describes the activity for the purposes of CEQA (CEQA Guidelines § 15168). Accordingly, pursuant to Government Code Section 65457, it is anticipated that no additional EIR or negative declaration will be required for “any residential project, including any subdivision, or any zoning change that is undertaken to implement and is consistent with a specific plan.”

Additional CEQA compliance is required if, after the adoption of the specific plan, one or more of the following events occurs: (1) Substantial changes are proposed in the project which require major revisions to the EIR; (2) Substantial changes to the circumstances under which the development project is being undertaken which require major revisions to the EIR; or (3) New information, which was not known and could not have been known at the time the EIR was certified, becomes available. If any of these events occurs, certification of a subsequent EIR, supplemental EIR, an addendum to the EIR, or adoption of a negative declaration, would be required depending on the environmental impacts identified.

## 1.4 Specific Plan Goals and Objectives

The following Specific Plan Goals and Objectives form the physical, economic, and environmental framework upon which the Specific Plan is built.

- Goal:** Accommodate a range of land uses and maintain a level of variety and flexibility that will allow the plan to adjust to market conditions and meet the economic, housing, and service needs of the community.
- Provide uses that ensure long-term economic benefits and stability for the City of Indio.
  - Develop a diverse set of land uses that permit a mix of commercial, industrial, and residential uses.
  - Implement employment-generating land uses that would create new jobs for the City of Indio.
  - Establish development standards and guidelines that facilitate business and employment opportunities, are adaptive to changing market conditions, and achieve local planning objectives.

**Goal:** Where industrial uses are near existing and planned residential development, ensure that industrial projects be designed to limit the impact of truck traffic, air, and noise pollution.

- Create a high-quality development that provides industrial land use activity adjacent to I-10.
- Encourage site plan designs to provide appropriate setbacks and/or design features that reduce negative impacts at the source.
- Encourage industrial land uses that accommodate buildings with loading bays that have a short direct access route to I-10 on- and off-ramps.
- Design safe and efficient vehicle access to commercial land uses from arterial streets to ensure efficient vehicular ingress and egress.
- Utilize existing designated truck routes and develop a safe and efficient system for delivering goods and services.

**Goal:** Ensure that development has a positive effect on the City of Indio and a high quality of life for all residents.

- Ensure development along Avenue 42 results in a connected, complete place with housing and commercial uses organized into a walkable, transit-supportive pattern of neighborhoods and centers.
- Establish development standards along Avenue 42 to include sidewalks, trees, landscape, pedestrian furniture, safe pedestrian crossings, and direct connections to the front entrances of retail and services.
- Provide landscaping that improves the streetscape experience and is supportive of the site's character and the character of the surrounding community.
- Encourage a mix of local- and regional-serving uses along Avenue 42 and Monroe Street that serves the needs of the community.
- Promote compatibility with surrounding uses through site design that provides appropriate transitions and buffer space.

**Goal:** Ensure the circulation network provides a safe and efficient level of connectivity for commercial and passenger vehicles, bicyclists, and pedestrians.

- Establish pedestrian connections between residential and retail commercial developments.
- Develop sidewalks for pedestrian use where existing gaps exist.
- Establish the opportunity for a Multi-Modal Transportation Network that serves all users and modes in a healthy, equitable manner.

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- Encourage commercial/retail developments to have common driveways to minimize the number of curb cuts along any given block to improve pedestrian safety.
- Encourage residential and non-residential developments within a ½ mile radius of existing or planned transit stops, particularly along Avenue 42, to facilitate and take advantage of transit service, reduce vehicle trips, and allow residents without private vehicles to access services.

**Goal:** Provide adequate infrastructure, services, and utilities to meet the needs of the community.

- Ensure adequate infrastructure and utility service levels before approving new development.
- Require new development to pay their fair share for required improvements to public services and infrastructure.

**Goal:** Provide a stand-alone regulatory instrument that will plan for the orderly and efficient buildout of the Specific Plan Area

- Establish appropriate land uses and development standards that optimize the Specific Plan Area potential and facilitate the achievement of project objectives.
- Establish appropriate land uses and development standards within the Airport Influence Area consistent with the densities, intensities, prohibited uses, and other development conditions defined in the Bermuda Dunes Airport Land Use Compatibility Plan.

## 1.5 Specific Plan Organization

The Oasis at Indio Specific Plan is organized into five Chapters and three appendices. The following describes each chapter and accompanying appendices:

- *Chapter 1 Introduction* - Provides local and regional context and setting; background; authority to prepare; relationship to other plans and documents; CEQA compliance; project goals and objectives and organization of the Specific Plan.
- *Chapter 2 Development Plan* – Explains the conceptual land use plan for the Specific Plan Area; circulation and mobility; grading; circulation, infrastructure, and public services.
- *Chapter 3 Development Regulations* – Identifies allowable uses, development intensities, and the general development standards, parking, and other regulations for development with The Oasis at Indio Specific Plan.
- *Chapter 4 Design Guidelines* - Provides design concepts and establishes design guidelines for development in the Specific Plan Area to ensure consistency with the vision and objectives of The Oasis at Indio Specific Plan.
- *Chapter 5 Administration and Implementation* – Discusses the development review procedures applicable to the Specific Plan Area; Implementation of the proposed land uses, including Specific Plan adoption, subsequent approvals and plans, substantial conformance findings, and phasing are outlined in this chapter. Additionally, financing sources and maintenance responsibilities are identified and administrative procedures regulating modifications and amendments.

- *Appendix A: Airport Land Use Commission (ALUC)* – Includes the ALUC approval letter, conditions of approval, and Airport Notice. Also included are the land use zones and associated land use restrictions as outlined in the 2004 Riverside County Airport Land Use Compatibility Plan (ALUCP) for Bermuda Dunes Airport.
- *Appendix B: Track Implementation Form* – Provides a mechanism for tracking Development Project Reviews/Entitlements.

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# Chapter 2 – Development Plan

The greatest value offered by The Oasis at Indio is its location. The property is located immediately north of Interstate I-10, a regional transportation route; south of Avenue 42, which serves as a continuous east-west multi-modal transportation corridor within North Indio; and west of Monroe Street, which provides direct access to/from I-10. The Specific Plan Area is located just over one mile east of the Bermuda Dunes Airport. When creating a guiding vision for The Oasis at Indio the property's setting and proximity to major transportation corridors and facilities were given the highest consideration.

The Oasis at Indio establishes a mixture of employment and residential land uses arranged sensible and efficiently that allows easy access and complements the surrounding community. To that end, The Oasis at Indio's land use plan is a contemporary mixed-use center containing industrial, commercial, and multi-family residential land uses that take advantage of the property's location near regional transportation corridors.

The Oasis at Indio is where businesses can locate and thrive and where multi-family housing opportunities are within walking distance of shopping, goods and services, employment opportunities, and public transit. The mixture of uses and their orientation on the property are designed for the benefit of the residents, visitors, the surrounding community, and the region.

Please note: Exhibits and illustrations are conceptual and are preliminary included to show intent. Implementing plans will require further refinement and approval.

## 2.1 Land Use Concept Plan

As shown in Figure 2-1, *Land Use Districts*, The Oasis at Indio Specific Plan specifies two primary land use districts: Mixed-Use, which allows residential and commercial uses, and Industrial. For planning purposes, the approximate 183-acre Oasis at Indio Specific Plan property is divided into four Planning Areas (PA) for development and an area reserved for a possible future electric substation.

Figure 2-1: Land Use Districts

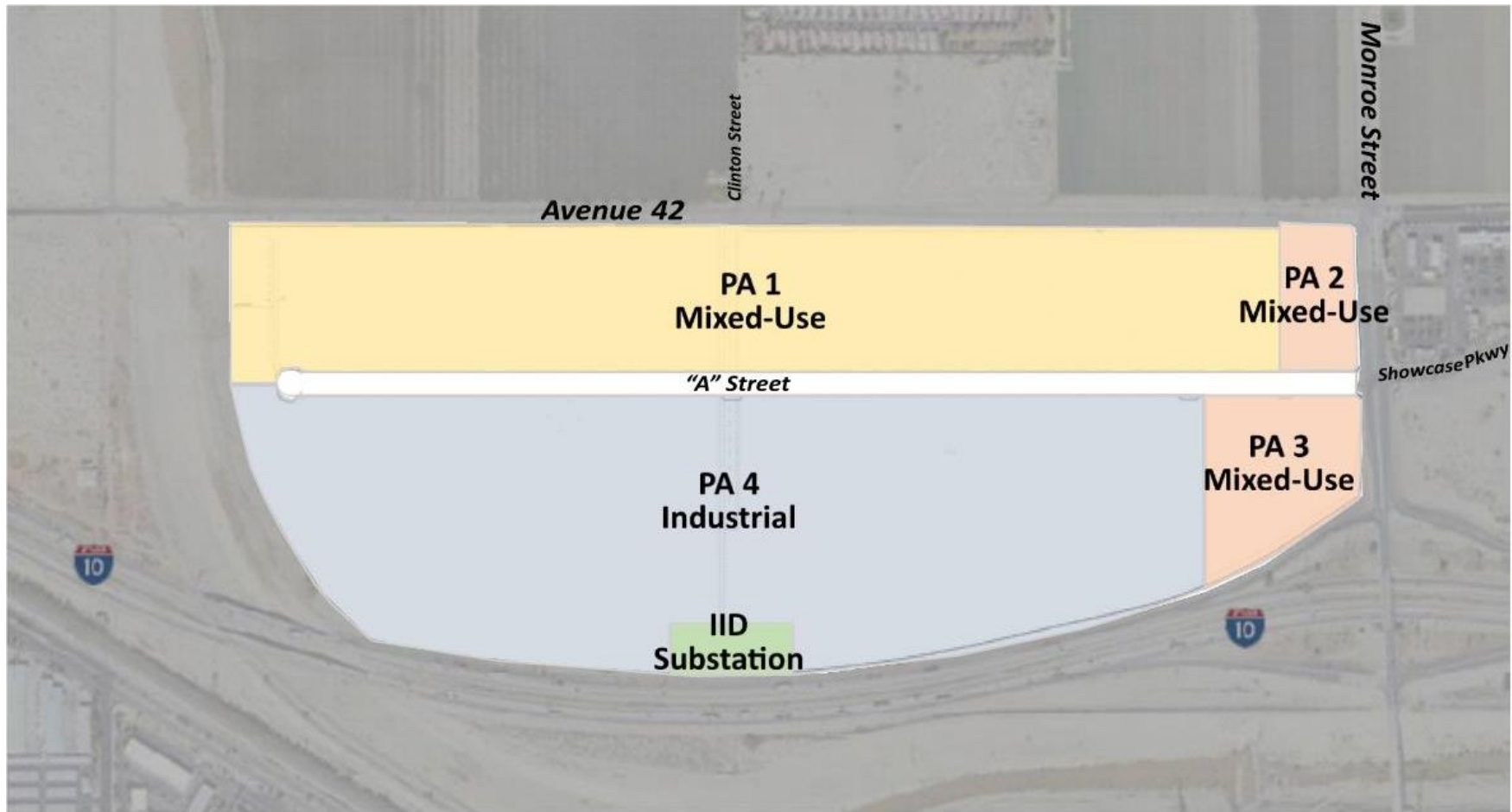


Table 2-1, *Land Use Plan Statistical Summary*, provides a high-level summary of the planned land uses and permitted intensity of uses within the planning areas of the Specific Plan. Table 2-1 reflects generally applicable standards and does not describe potential buildout, is further controlled by the development maximum reflected in Table 2-2 and the land use equivalency conversion ratios identified in Specific Plan Section 5.2.7, *Land Use Conversion*. The applicable development standards in Specific Plan Section 3.5, *Development Standards*, also apply to any future development within the Specific Plan’s Planning Areas. Each Planning Area may develop a singular anticipated use or as a combination of uses identified below in a horizontal or vertical mixed-use development.

Further definition of each planning area will occur as individual developments are implemented through the Planning Review process and based on market demand.

Table 2-1 Land Use Plan Statistical Summary						
Land Use District	PA	Acreage (Approximately)	Allowed Uses <sup>1</sup>			
			Multi-Family Residential		Non-Residential	
Mixed-Use	1	61.6	20-50 DU/AC		0.50 FAR	
Mixed-Use	2	4.2				
Mixed-Use	3	9.3				
Industrial	4	92.8	N/A		0.60 FAR	
Public ROW Dedication		2.0	N/A			
I.I.D Substation		2.4	N/A			
Interior ROW		10.5	N/A			
<b>TOTALS</b>		<b>182.8</b>	-	-	-	-

1. The “Allowed Uses” and associated intensities are reflective of the Development Standards applicable to each Planning Area district. It does not reflect maximum buildout, however, as development within the Specific Plan is also limited to the intensity identified in Table 2-2, subject to the land use equivalency ratios identified in Specific Plan Section 5.2.7. There is no equivalency ratio for industrial uses and residential or commercial square footage capacity cannot be converted to additional industrial square footage.

### 2.1.1 Specific Plan Development Limit

Notwithstanding the Specific Plan’s permitted densities/intensities and development standards, the development capacity limitations below also control maximum development (buildout) within the Specific Plan. It is important to note that while the equivalency tables allow variation in intensity between uses, the maximums identified below reflect the most intensive development that could occur within the Specific Plan. Any development variants that could occur using equivalency would be consistent with and within the scope of the maximums.

Table 2-2 Development Potential Maximums				
Land Use District	PA	Acreage (Approximately)	Maximum Potential	
			Multi-Family Residential	Commercial Non-Residential Square Footage
Mixed-Use	1	61.6	50 DU/AC	20,000 square feet
Mixed-Use	2	4.2		
Mixed-Use	3	9.3		
Industrial	4	92.8	N/A	1,806,290 square feet
Public ROW Dedication		2.0	N/A	
I.I.D Substation		2.4	N/A	
Interior ROW		10.5	N/A	
<b>TOTALS</b>		<b>182.8</b>	<b>3,240 DU</b>	<b>1,826,290</b>

The “maximums” identified in Table 2-2 reflect the most intensive development that could occur within the Specific Plan. However, the exact development may not reflect the uses developed. The square footage of commercial non-residential uses may increase while development of residential units would decrease, for instance, consistent with the land use equivalency conversion ratios in Section 5.2.7.

### 2.1.2 Planning Area 1-3

Planning Areas 1, 2, and 3 comprise approximately 75.1 acres in the northern and northeastern quadrant of the Specific Plan Area. As shown on Figure 2-1.

- *Planning Area 1* is bordered on the north by Avenue 42; on the west by the Thousand Palms Stormwater Channel; on the east by PA 2; and on the south by an internal east-west roadway (“A” Street);
- *Planning Area 2* is bordered on the north by Avenue 42; on the east by Monroe Street; on the south by “A” Street; and on the west by PA 1; and
- *Planning Area 3* is bordered on the north by “A” Street; on the east by Monroe Street; on the south by the I-10 on-ramp; and on the west by PA 4.

The mixed-use designation allows for medium to high-density multi-family residential and commercial land uses in a vertical or horizontal mixed-use development within walkable distance to surrounding neighborhoods, a variety of nearby retail, commercial centers, workplaces, public transit, and the Coachella Valley alternative transportation corridor (CV Link). Multi-family residential housing will contribute to a diverse set of land uses, ensuring a mixed-use development and a variety of housing types and densities within a walkable urban format of The Oasis at Indio Specific Plan and the surrounding area.

Residential development is envisioned to include private on-site amenities such as fitness rooms, community pools, clubhouses for the residents and their guests, common open space areas, walkways, landscape parkways and buffer areas, and parking. The final site plan and housing type will largely depend on future developers.

Planning Areas 2 and 3 front Monroe Street between Avenue 42 and the westbound I-10 onramp. This prominent location affords the opportunity to attract and provide a high-quality and viable balance of commercial uses and moderate- to high-intensity multi-family residential housing. The development of PA 2 would provide employment opportunities and various retail services such as restaurants, shops, and other amenities within walking distance of existing nearby neighborhoods and future residents of the project.

Designed to complement the uses of PA 2, Planning Area 3 would allow for a range of commercial uses that benefit from visibility and access from the I-10 Freeway. A hotel would be allowed in the mixed-use zones to serve the surrounding community and region, such as visitors to the nearby Polo Club, Acrisure Arena, annual music festivals, and Bermuda Dunes Airport. For a list of permitted uses and additional information, refer to Chapter 3, Section 3.4, Permitted Uses, and Section 3.5, Development Standards.

Frontage along Avenue 42 would provide parkways containing sidewalks, shade trees, and landscaping to enhance the pedestrian experience and is supportive of the site's character and the character of the surrounding community. See Chapter 4, *Design Guidelines*, for the placement, orientation, and design along Avenue 42.

A landscaped transition zone is planned along the southern boundary of PA 1 to soften the overall site design, support compatibility with surrounding land uses, and serve as a water quality/retention basin as part of the development's drainage system. See Figure 2-6.

### 2.1.3 Planning Area 4

Encompassing approximately 92.8 acres in the southern portion of the Specific Plan, Planning Area 4 is the largest of the planning areas and is designated for industrial land use. As shown in Figure 2-1, PA 4 is bordered on the north by "A" Street, on the east by PA 3, on the south by the I-10 freeway, and on the west by the Thousand Palms Stormwater Channel.

Planning Area 4 is designed to accommodate buildings of various sizes that house users such as general light industrial, manufacturing, and warehouse/distribution operations. These types of uses typically attract both passenger car and truck traffic that benefit from a short, direct route to the regional transportation (freeway) system.

The buildings in PA 4 are configured to provide a sustainable design with proper balance in size, aesthetics, energy efficiency, accessibility, safety, and security. Enhanced landscaping and decorative screen walls along the northern boundary of PA 4 provide an additional buffer treatment to further soften the transition and reduce potential impacts to the uses north of PA 4. A potential Imperial Irrigation District (I.I.D) substation and water quality/detention basins are proposed along the southern portion of PA 4. The water quality/detention basins provide additional green space as part of the development's storm drain system. "A" Street provides east-west vehicular access from industrial uses

## Chapter 2 Development Plan

in PA 4 to the existing signalized intersection at Monroe Street with immediate access to the Interstate 10/Monroe Street on- and off-ramps.

Note: The site design represented in Figure 2-2 is conceptual in nature and exemplifies one possible design scenario that has been developed in accordance with the design criteria of The Oasis at Indio Specific Plan and may not represent the ultimate use or building layout within the Specific Plan, which will be determined with future site-specific development proposals.

## 2.2 Circulation and Mobility

The main objective of the Circulation Plan is to provide direct, safe, and convenient vehicular and non-vehicular access for residents, visitors, and employees to, from, and through, the Specific Plan's four Planning Areas.

Development of The Oasis at Indio requires improvements to existing perimeter public streets and "A" Street, an east-west internal private roadway. The project will amend the City's General Plan to declassify Clinton Street, an unbuilt collector road. The internal roadway is designed to coordinate the movement of vehicles and pedestrians, minimize potential conflicts, and encourage a safe and enjoyable pedestrian experience in compliance with the General Plan.

### 2.2.1 Vehicular Circulation

The circulation system depicted in Figure 2- 3, *Conceptual Vehicular Circulation and Mobility Plan*, provides the necessary roadway improvements and design to accommodate the Land Use Plan, provides mobility choices to all residents, and limits truck traffic on Avenue 42.

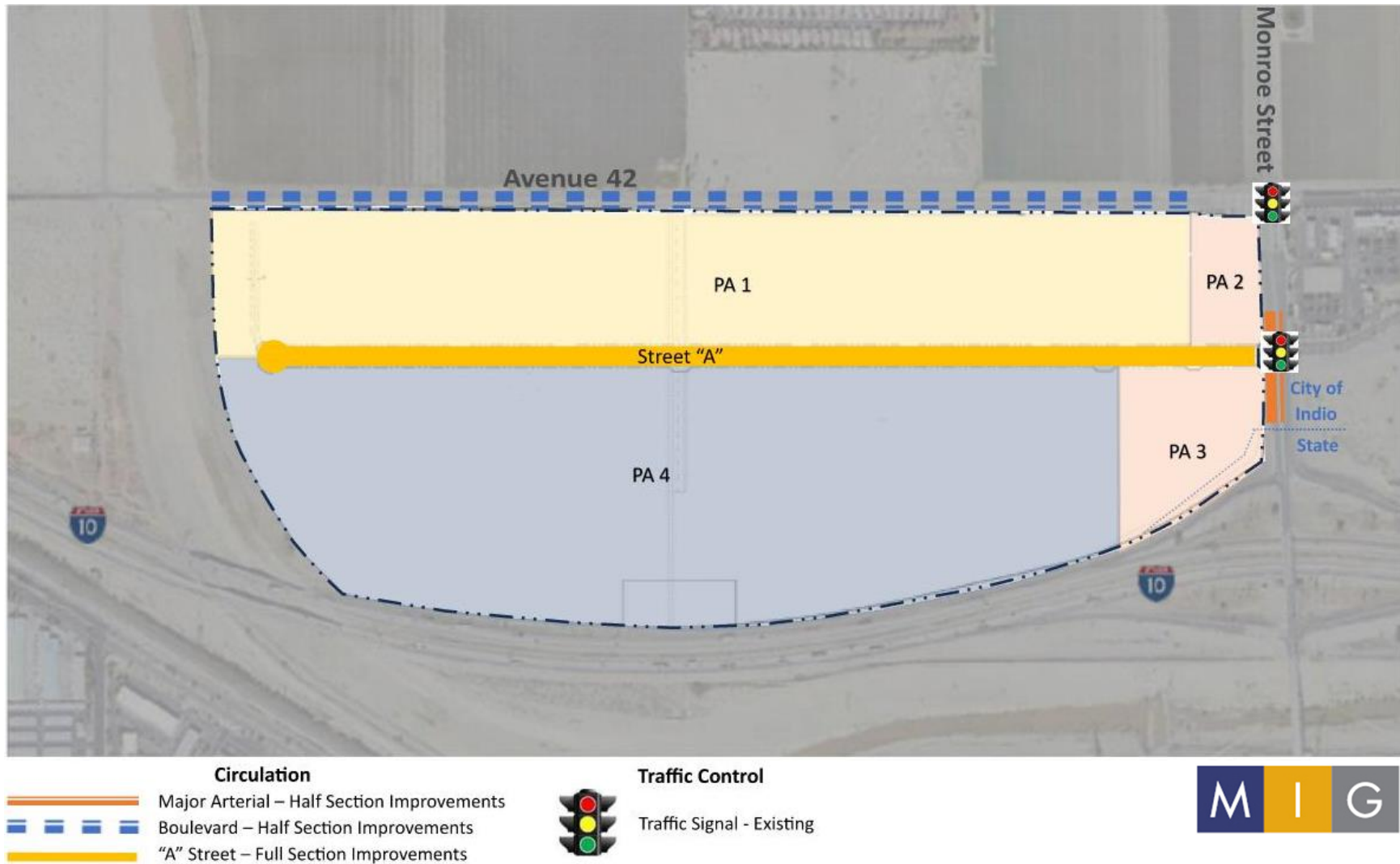
As a component of this project, a comprehensive traffic analysis was prepared. This analysis identified existing conditions, forecasted future conditions, and provided recommended roadway segment and intersection improvements to address project-related significant impacts associated with the build-out of the anticipated land use plan. This analysis is provided in the technical appendices of The Oasis at Indio Specific Plan EIR.

Monroe Street serves the Specific Plan to the east and Avenue 42 to the north. Concurrent with the development of The Oasis at Indio, improvements to Monroe Street and Avenue 42 will result in completed street half-sections consistent with the City's General Plan Mobility Element as may be refined by the City Engineering Department. All street improvements will be constructed in accordance with applicable City of Indio design standards and specifications.

Figure 2-2: Conceptual Land Use Plan



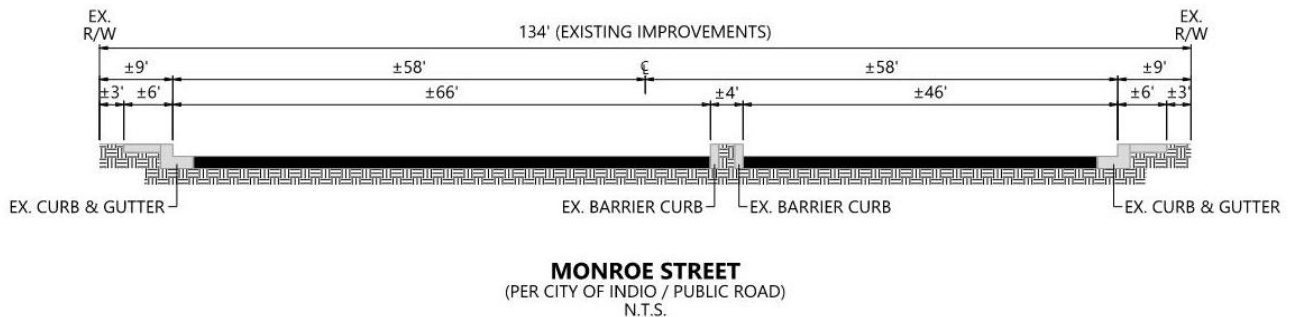
Figure 2- 3: Conceptual Vehicular Circulation and Mobility Plan



Interstate 10, which is under the authority of CalTrans, is located immediately south of the Specific Plan Area, forming the southern boundary of Planning Areas 3 and 4. A portion of land at the southeast corner of the Specific Plan Area has been identified by CalTrans for expanded interchange improvements along I-10/Monroe Street westbound on-ramp. Approval of The Oasis at Indio Specific Plan does not preclude CalTrans’ plans for the Monroe Street interchange improvements or the operation and maintenance of Interstate 10. Drivers traveling between the Specific Plan Area and the freeway system will have convenient access via the on- and off-ramps at Monroe Street, Jackson Street (one mile east), and Jefferson Street (two miles west).

### 2.2.1.1 Monroe Street

Monroe Street forms the eastern boundary of the Specific Plan and consists of two lanes in each direction, with signalized turn lanes eastbound onto Showcase Parkway (existing) and westbound onto Avenue 42. Additional features include a non-landscaped barrier curb, curb-adjacent sidewalks, and streetlights within the public right-of-way. Monroe Street is classified in the City’s General Plan Mobility Element as a 6-lane Major Arterial with vehicular efficiency prioritized. Bicycle lanes can be provided and further enhanced or complemented by other facilities or off-street pathways. As shown below, the existing segment of Monroe Street between Showcase Parkway and Avenue 42 is currently improved with an ultimate right-of-way (ROW) width of 134 feet with 116 feet of pavement, including a four-foot-wide barrier curb and curb and gutter improvement, and a nine-foot-wide parkway on both sides of the street. On-street parking is not allowed. Monroe Street is a designated truck route within the General Plan. Improvements along Monroe Street may be required in conjunction with the implementation of the Specific Plan.



As part of the I-10 Freeway improvements, Caltrans proposes to widen Monroe Street, South of Showcase Parkway, to incorporate an expanded median and two additional through traffic lanes, as well as relocating the dedicated right turn lane to the westbound freeway on-ramp.

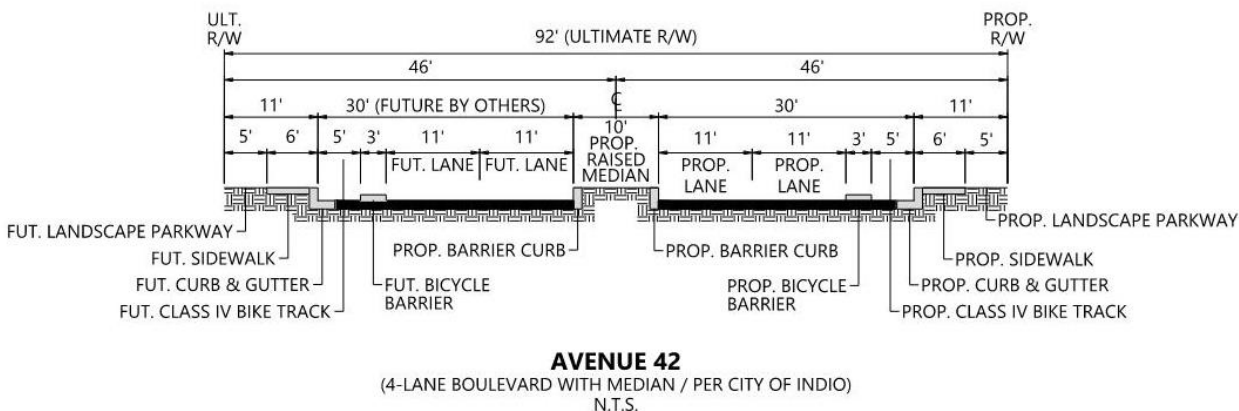
### 2.2.1.2 Avenue 42

Avenue 42 currently consists of one paved lane in each direction with above-ground utility poles along the Specific Plan Areas frontage and an unimproved dirt median east of Clinton Street. Avenue 42 is a designated truck route within the General Plan.

As shown below, Avenue 42 would feature an ultimate 92-foot-wide right-of-way with 44 feet of pavement, a 10-foot-wide raised median, curb and gutter improvements, a five-foot Class IV bike lane,

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and an 11-foot-wide parkway on both sides of the street. Intersection improvements for pedestrians should be emphasized to improve comfort, safety, connectivity, and accessibility. Half street improvements along the southern portion of Avenue 42 abutting the specific plan frontage would be required in conjunction with the development of the Specific Plan.



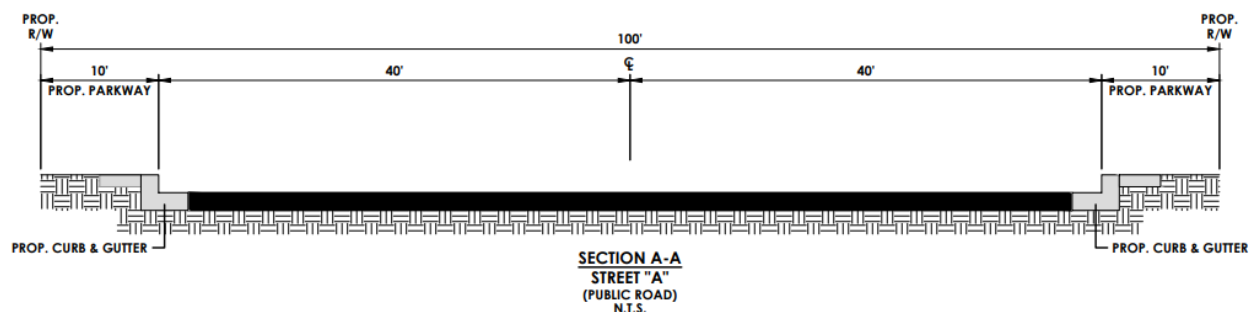
### 2.2.1.3 Clinton Street

The Mobility Element of the General Plan does not describe Clinton Street as an arterial or a collector but does depict it extending southerly across Avenue 42 in a north-south direction through PA 1 of the Specific Plan Area. Removal of this roadway within the project boundary is part of the General Plan Amendment.

### 2.2.1.4 Internal Circulation

An east-west private collector street ("A" Street) provides internal circulation through the Specific Plan Area. "A" Street will connect to the existing signalized intersection on Monroe Street and would provide primary access for PA 2, PA 3, and PA 4, and secondary access for PA 1. Driveways on the south side of the collector street are left-in/right-out only providing commercial trucks a direct route to/from the I-10 freeway via Monroe Street, limiting truck traffic on Avenue 42.

As shown in the cross-section below, "A" Street features a 100-foot-wide right-of-way with 80 feet of pavement, curb and gutter improvements, and a 10-foot-wide parkway on both sides of the street.



## 2.2.2 Non-Vehicular Circulation

A comprehensive network of pedestrian and bikeway facilities are provided within The Oasis at Indio as shown in Figure 2-4, *Conceptual Non-Vehicular Circulation and Mobility Plan*. The purpose for the non-vehicular circulation plan is to provide a Complete Street that incorporates a pedestrian- and bicycle-friendly environment and enhance connectivity across all travel modes to encourage the use of non-vehicular modes of travel.

### 2.2.2.1 Bikeways

Roadway improvements along Avenue 42 and Monroe Street will include a Class IV Separated Bikeway or Cycle Track – currently, no bike path or lane exists. The Class IV Separated Bikeway or Cycle Track provides a delineated right-of-way with a physical separation or barrier between bicyclists and motor vehicle traffic. This separation can include parked vehicles, bollards, curbs, or any other physical device that provides this separation.

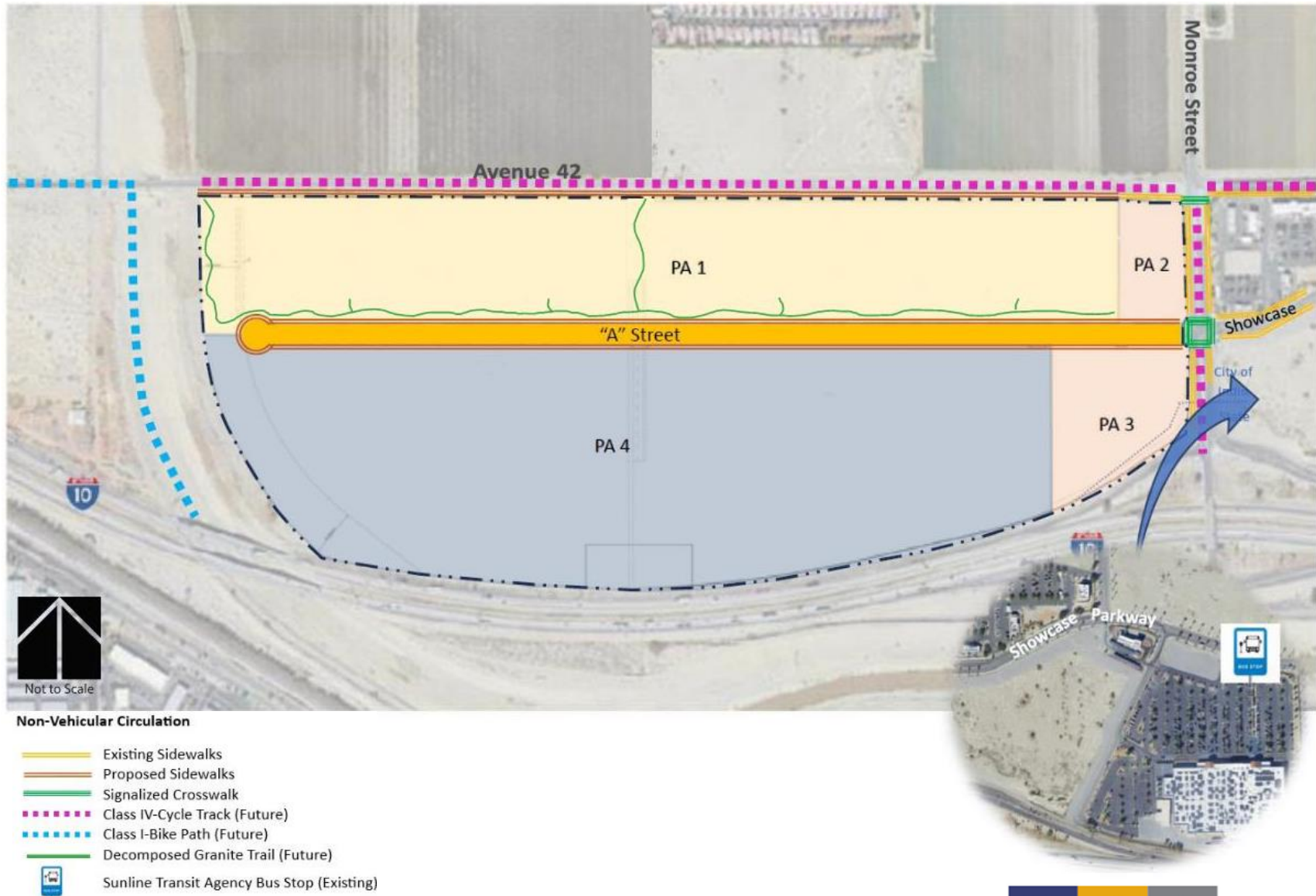
The Thousand Palms Stormwater Channel abutting, the Specific Plan’s westerly boundary, provides a Class 1 – Bike Path and serves as a community connector to the Coachella Valley alternative transportation corridor (CV Link) – a 50-mile pathway for bicycles, pedestrians, and low-speed (up to 25 mph) electric vehicles that follows the general alignment of the Coachella Valley Stormwater Channel from Palm Springs to Coachella. The Class I Bike Path provides a separate corridor not served by streets and highways and is away from the influence of parallel streets. Class I bikeways are for non-vehicle use only, with opportunities for direct access and recreational benefits, right-of-way for the exclusive use of bicycles and pedestrians, and crossflow conflicts are minimized.

### 2.2.2.2 Pedestrian ways

Existing sidewalks are located on the east and west sides of Monroe Street. Currently, there are no sidewalks along Avenue 42 west, of Monroe Street.

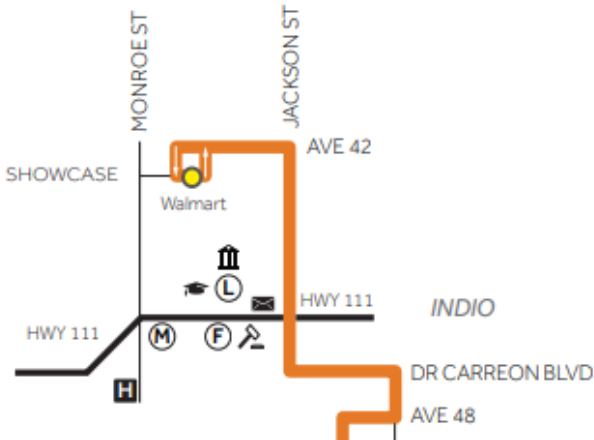
The southside widening of Avenue 42 will include a pedestrian sidewalk. The development of “A” Street will include sidewalks on both sides of the roadway. In addition, a decomposed granite trail will parallel along the north side of “A” Street in the landscape transition zone. Streetscape elements, including trees and landscaping, will be an important component of the Complete Streets design.

Figure 2-4: Conceptual Non-Vehicular Circulation and Mobility Plan



### 2.2.3 Public Transit

Sunline Transit routes provide regional connections to neighboring cities and local circulation routes within the City of Indio. The local transit northbound loop within the City of Indio is Transit Route No. 8 (formerly Routes 80, 81, 90 & 91). An accessible bus stop with service on Transit Route No. 8 is located on Showcase Parkway, approximately a quarter mile east of Monroe Street. The route operates in a counterclockwise direction along Jackson Street up to Avenue 42. Service frequency is 60 minutes. Travel time around the loop is approximately 50 minutes.



*Sunline Transit Route No. 8*

## 2.3 Grading

The site's natural topography is relatively flat. No unusual grading conditions are present, and substantial import or export of earth materials is not expected. The primary objectives of the grading plan are to provide stable development pads for construction, balance the cut and fill grading quantities on-site, and meet City of Indio building standards and acceptable infrastructure gradient requirements. The grading plan is shown in Figure 2-5, *Conceptual Grading Plan*.

## 2.4 Landscape and Green Space

The Oasis at Indio landscaping and green space plan aims to achieve visual appeal, soften site design, promote compatibility, provide screening, and control stormwater runoff. Mindful of the environment and California's resources, the Specific Plan Area incorporates a desert-friendly landscape palette with low-water usage in compliance with the City's Water Efficient Landscape Ordinance. Refer to Chapter 4, Design Guidelines, for detailed information on landscape design.

Figure 2-5: Conceptual Grading Plan

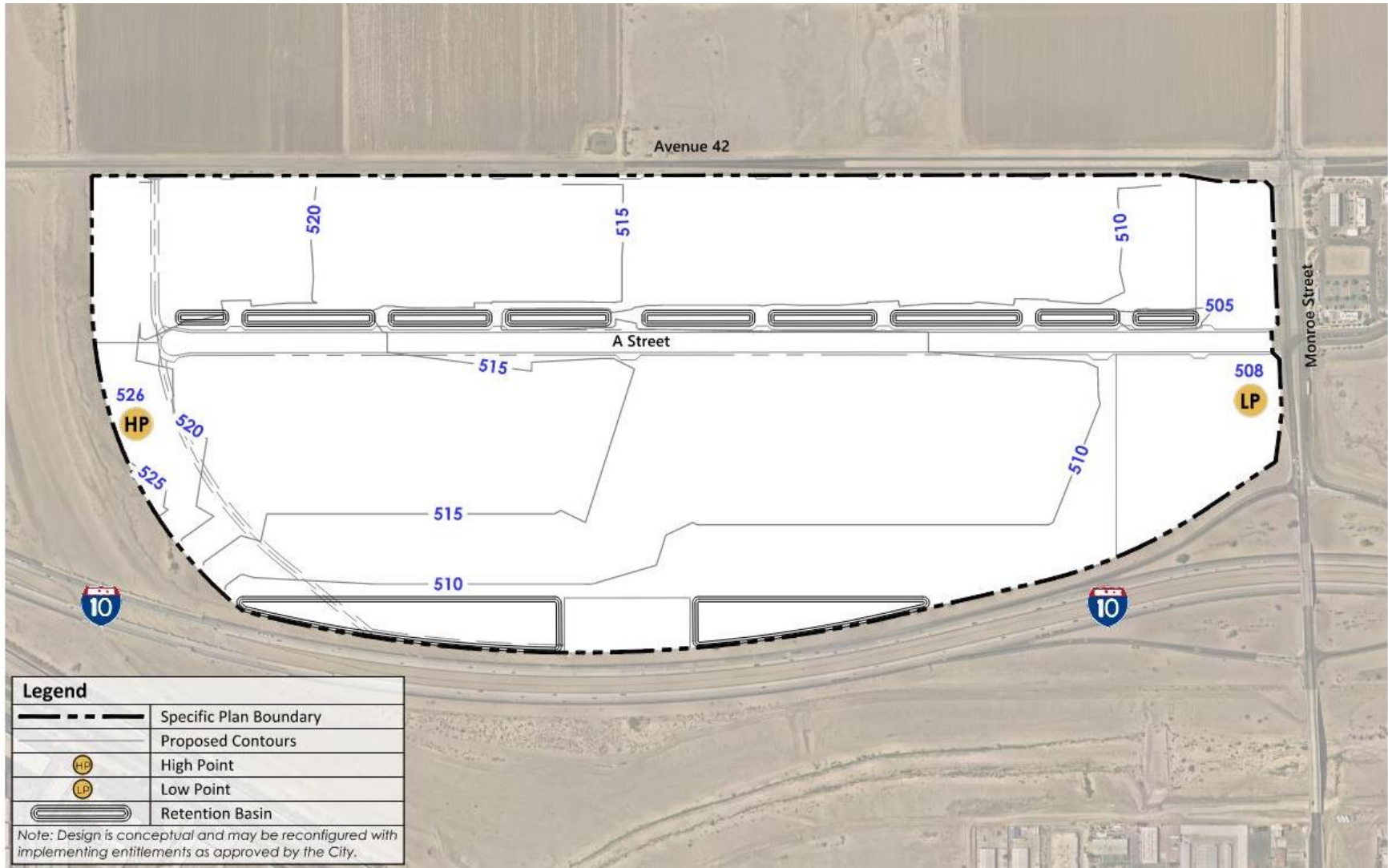
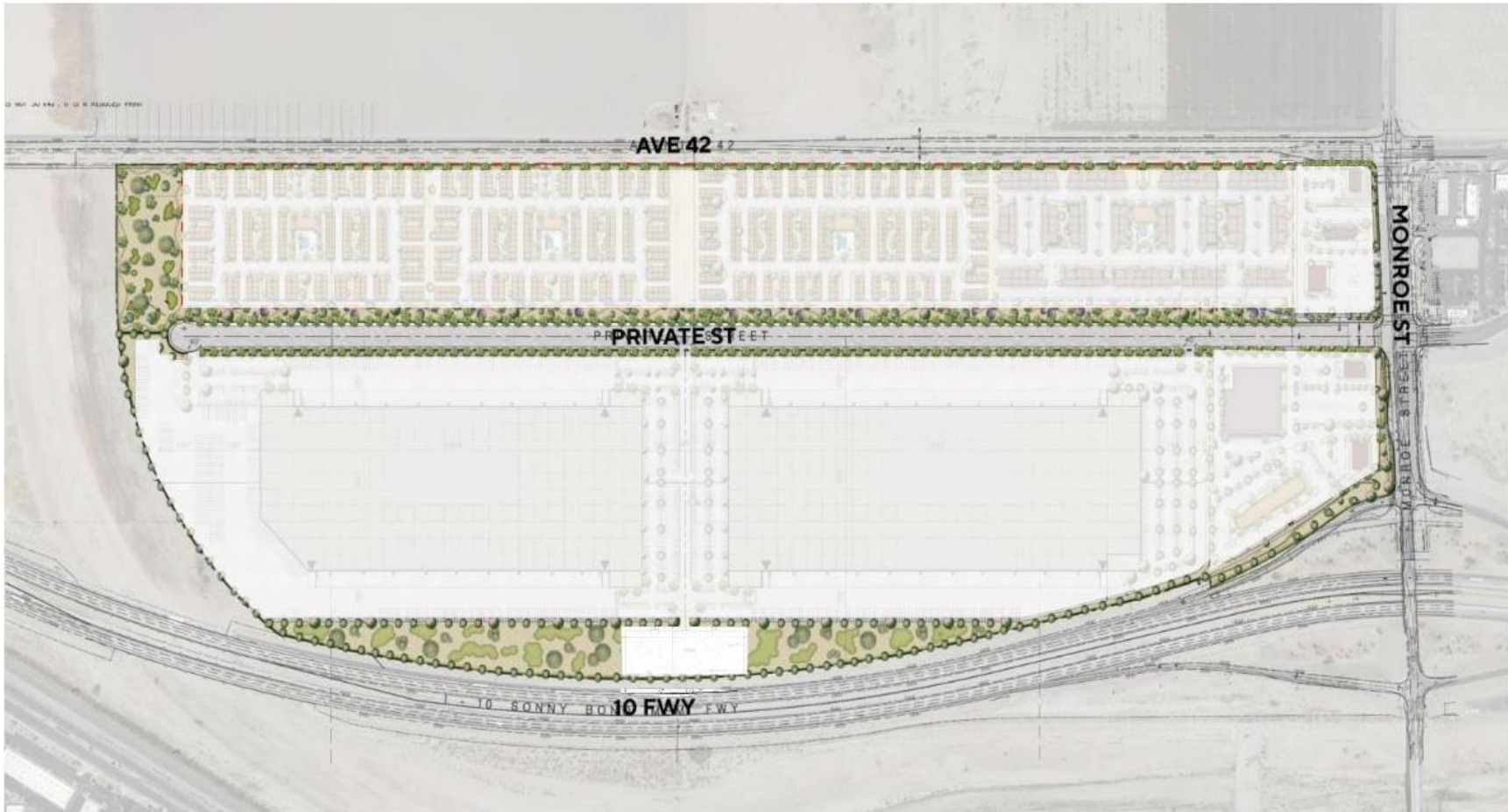


Figure 2-6: Conceptual Landscape and Green Space Plan



## Chapter 2 Development Plan

Landscaping occurs throughout the Specific Plan Area, but most prominently along the various roadways and site frontage entryways. Street corners include multiple layers of landscaping and identification monuments. Streetscape landscaping combines evergreen and deciduous trees, shrubs, and groundcovers to create a sustainable and visually pleasing experience for pedestrians and passing motorists. Creative solutions along the southern boundary of “A” Street include berms, multiple layers of landscaping, and the use of decorative walls. An enhanced landscape transition zone along the northern boundary of “A” Street would provide passive pedestrian uses and serve as a retention facility. See Chapter 4 Design Guidelines.

Retention basins along Street “A” would be planted with a living desert landscape palette utilizing layers of large, canopied evergreen shade trees, shrubs, and semi-tropical flowering plants to create a natural low-water landscape with a variety of shapes, intriguing color palettes, and textures.

The retention basins proposed along the southern boundary of PA 4 would create an open space buffer between the industrial use and Interstate 10.

## 2.5 Infrastructure and Utilities

Buildout of The Oasis at Indio Specific Plan requires the installation of water, sewer, drainage, and other utility infrastructure, as described in this section. All utility infrastructure improvements shall be constructed in accordance with applicable City of Indio design standards and specifications.

### 2.5.1 Water

The potable water provider for the Specific Plan Area is served by Indio Water Authority (IWA). As shown on Figure 2-7, *Water Infrastructure Plan*, an existing 24-inch water main underlying Avenue 42 and an existing 14-inch water main underlying Monroe Street will be used to serve the Specific Plan. A new public 18-inch water line will underlie “A” Street and underlie the access easements north of “A” Street connecting to the off-site water main that underlies Monroe Street and Avenue 42.

This project would be required to pay the IWA Water Impact Fees in addition to the Supplemental Water Supply Fees.

### 2.5.2 Wastewater

Valley Sanitary District (VSD) is the sanitary sewer provider for the Specific Plan Area. As shown on Figure 2-8, *Sanitary Sewer Infrastructure Plan*, an existing 27-inch sewer main at the intersection of Monroe Street and Avenue 42. As the specific plan builds out, sewer capacity in the adjacent off-site mains will need to be assessed. Improvements could include a new 12-18-inch sewer main in Monroe from the intersection of Avenue 42 into “A” Street or extending two mains, one 10-18 along the project frontage in Avenue 42 along with a 10-12-inch sewer main from Monroe along “A” Street. The ultimate sewer system required will be reviewed with VSD and will be based on the project needs. Sewer stub outs will be provided for future connections.

Figure 2-7: Conceptual Water Infrastructure Plan

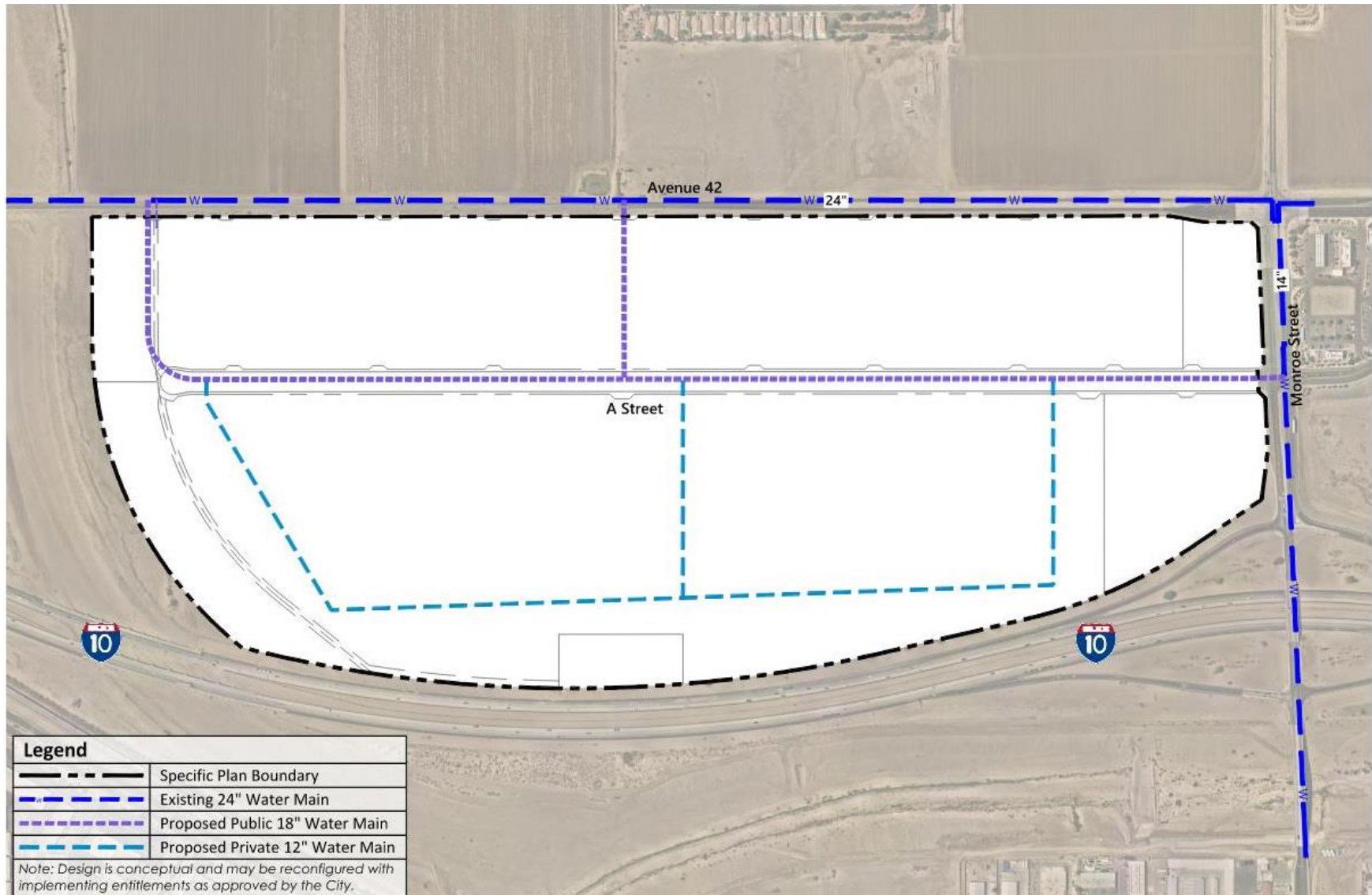
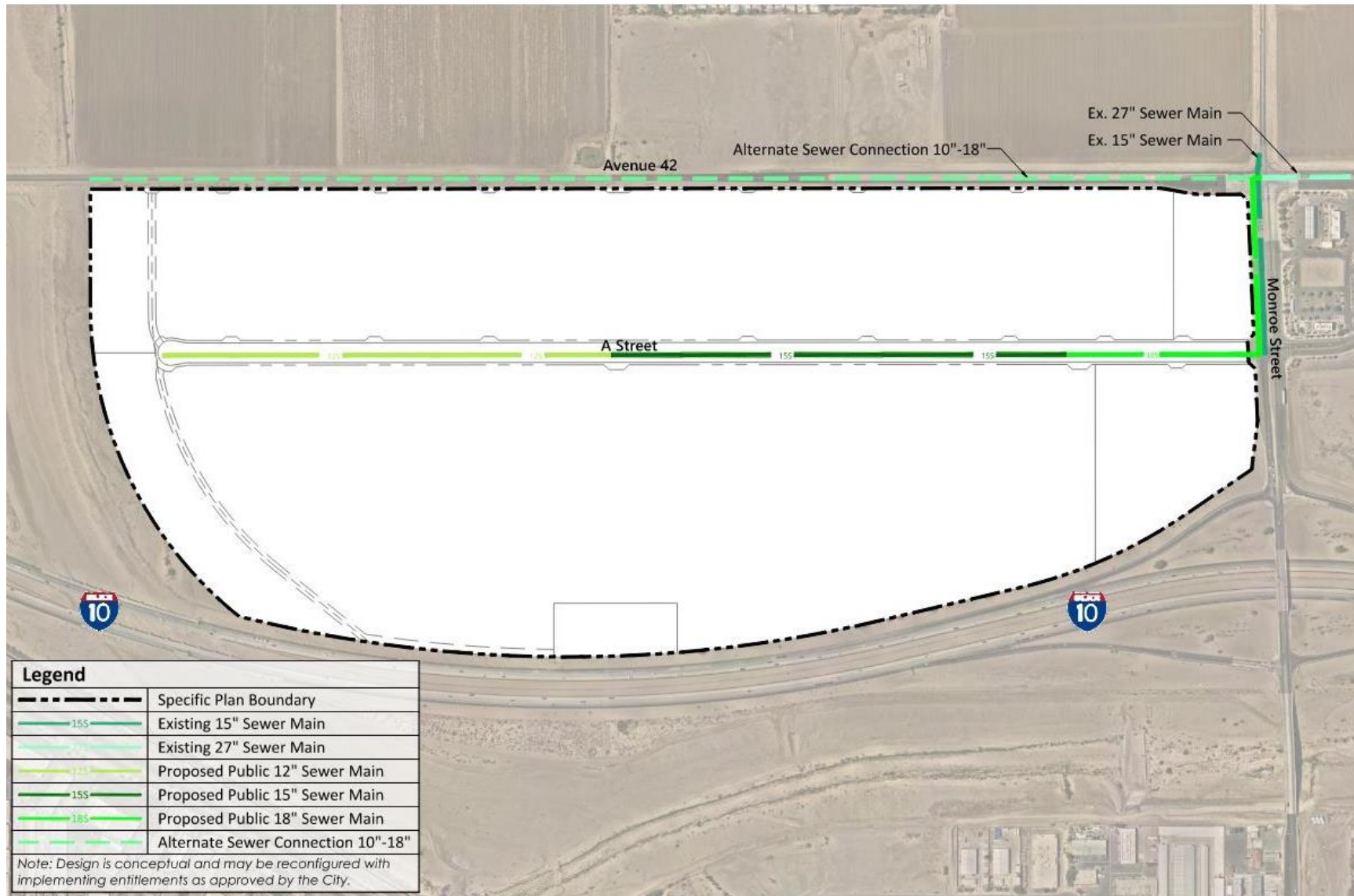


Figure 2-8: Conceptual Sanitary Sewer Infrastructure Plan



### 2.5.3 Stormwater Drainage and Flood Control

The City maintains local drainage facilities and generally directs runoff from local streets to regional stormwater channels, which are constructed, operated, and maintained by the Coachella Valley Water District (CVWD). The main regional flood control facility in the City of Indio is the Coachella Valley Stormwater Channel, which meanders through the City in a west-to-east direction, partially along the Interstate 10 Freeway. In addition, the Thousand Palms Stormwater Channel is located along the property's west boundary and directs stormwater from the Sun City Shadow Hills community. The Channel is currently unimproved, and CVWD is currently proceeding with the channel lining project. The local storm drain system consists of gutters, engineered storm drains, and channels.

Natural drainage of the site occurs from the northwest to the southeast. Each planning area will manage stormwater independently in conjunction with development. The drainage concept is to provide numerous site-specific pick-up points to channel storm water from non-permeable areas via a combination of private streets, overland flows, and storm drain systems and convey it to the nearest retention basin.

Retention basins are conceptually proposed along "A" Street and the southern boundary of PA 4. These retention basins are designed to comply with City engineering standards by providing sufficient capacity to contain the incremental increase in onsite flows (existing vs. developed condition) from a 100-year storm event rather than increasing or concentrating runoff to downstream properties. They will also serve a secondary function of capturing and containing pollutants that are washed off roads and driveways. See Figure 2-9, *Storm Drain Infrastructure Plan*.

### 2.5.4 Dry Utilities

Electric service for the Specific Plan Area is provided by the Imperial Irrigation District (IID). Existing overhead electric lines are located on above-ground utility poles and currently extend in an east-west direction along the south side of Avenue 42 and a north-south direction transecting through the Specific Plan Area. These overhead utility lines support both 12.5 kV local distribution and 92 kV regional transmission lines. In conjunction with the development of the Specific Plan Area, new conduits and switch vaults would be installed, and overhead utility lines may be undergrounded by the developer where appropriate based on size/capacity in accordance with applicable public utility standards and specifications and to the satisfaction of the City of Indio Public Works Engineer. Transmission lines will remain above ground due to their high voltage.

As part of the development, a crucial component is the proposed onsite power sub-station. This sub-station will not only provide power to the project but will also contribute excess power to the community grid. This surplus power will significantly enhance the resilience of the electric system in the area, ensuring a reliable and stable power supply for the community.

Natural gas for the Specific Plan Area is served by Southern California Gas (SCG) Company. As shown in Figure 2-10, *Dry Utility Infrastructure Plan*, an existing gas line underlies Monroe Street. New gas lines are planned to be installed to fully service the Specific Plan Area.

Figure 2-9: Conceptual Drainage Infrastructure Plan

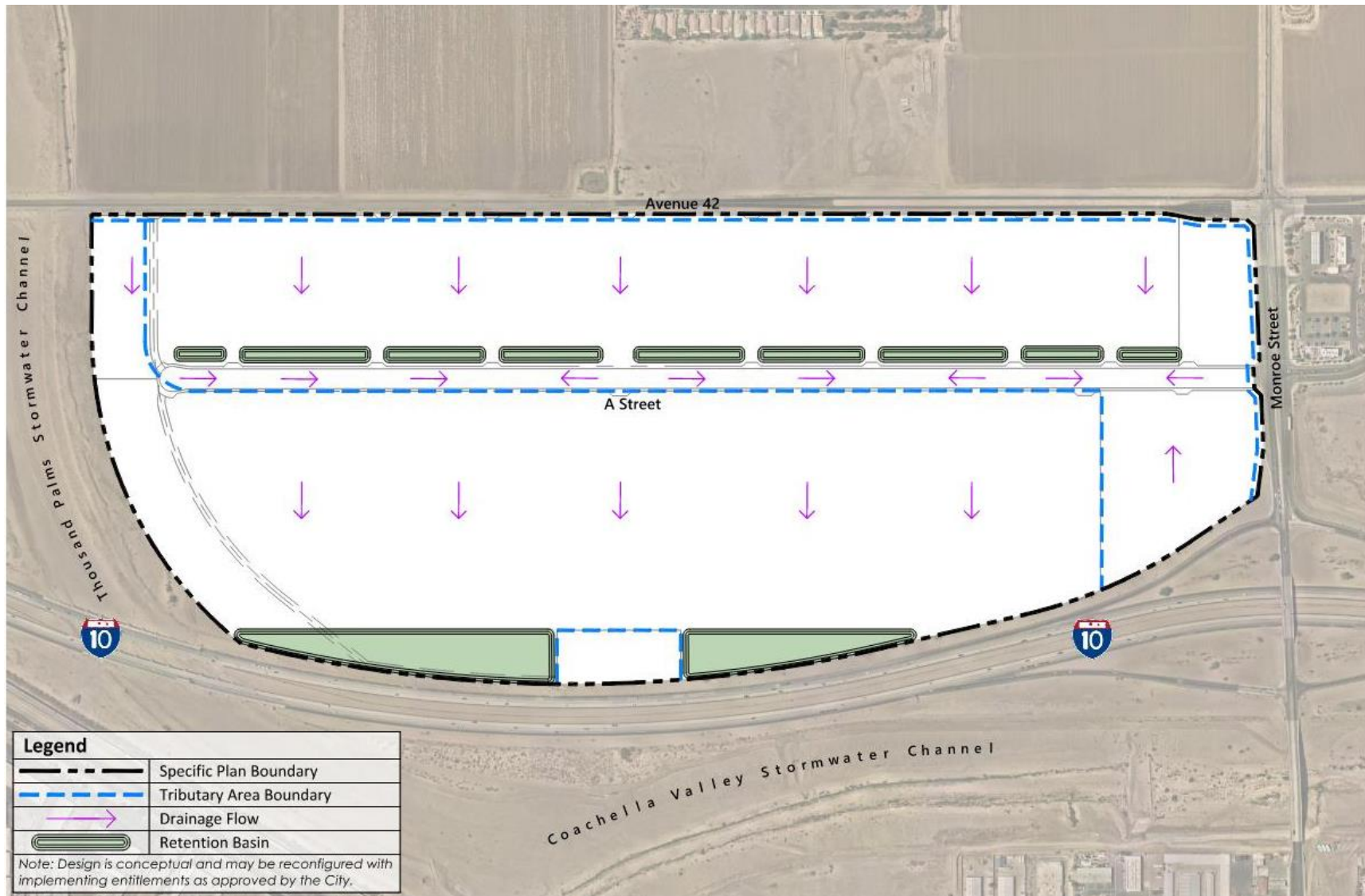
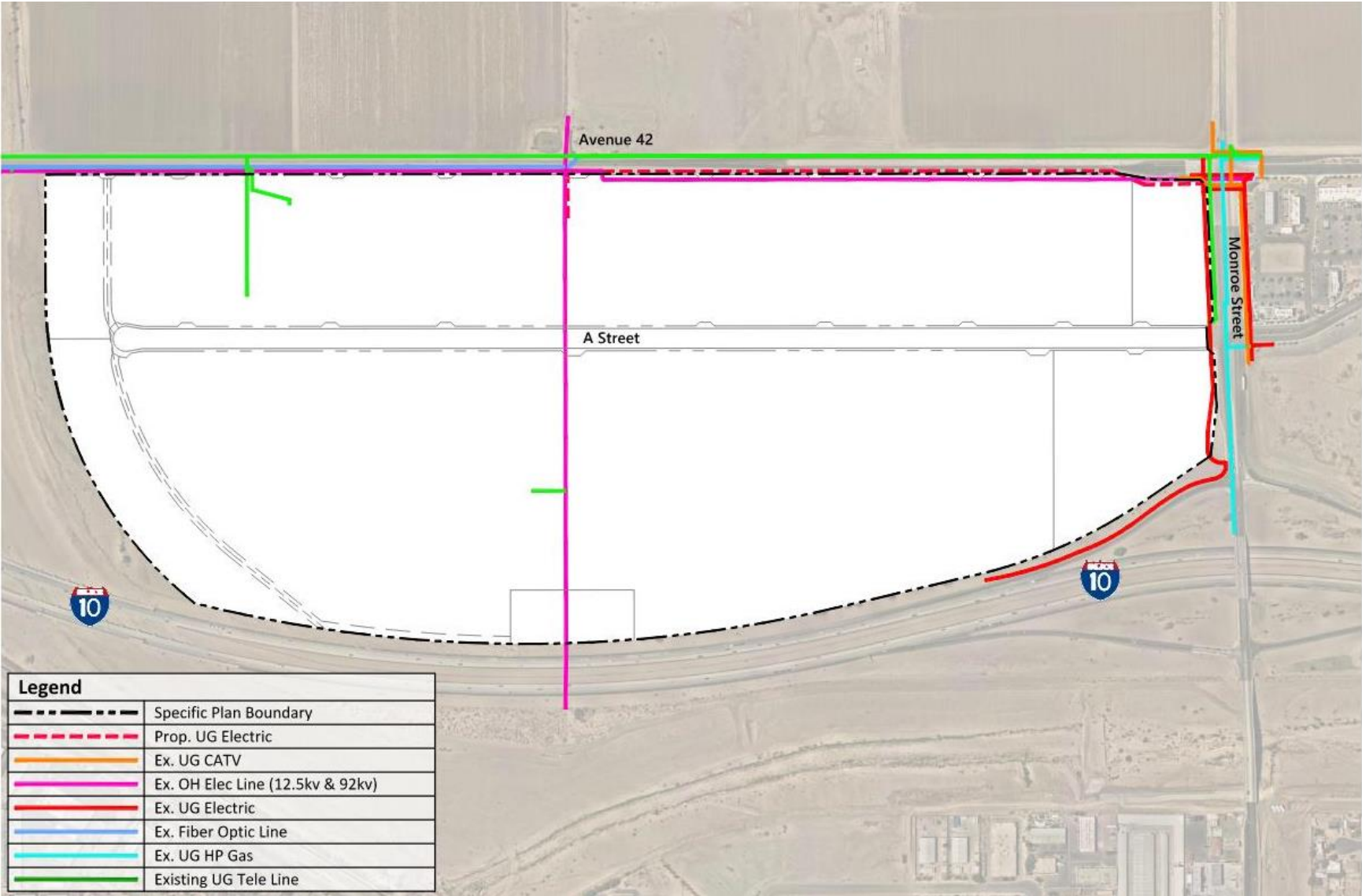


Figure 2-10: Conceptual Dry Utility Infrastructure Plan



## 2.5.5 Fiber Optics

The City of Indio has partnered with Magellan Advisors to develop a Fiber Master Plan to guide the development of reliable and cost-effective next-generation broadband. The Plan will develop strategies for enhancing broadband in the City of Indio to expand innovation and quality of life.

Connections to fiber optic networks do not currently exist within the Specific Plan Area. As the infrastructure needed to deliver these services is made accessible in the future, individual projects will have the opportunity to connect and make these services available.

## 2.5.6 Solid Waste

The City of Indio has agreements with Burrtec Waste and Recycling Services (Burrtec) for residential and commercial trash and recycling collection. Burrtec’s contract calls for solid waste to be transported to the Indio/Coachella Valley Waste Transfer Station in Coachella located approximately 5.5 miles southeast of the Specific Plan Area.

## 2.6 Public Services

### 2.6.1 Police

Police protection is provided by the City of Indio Police Department (IPD), located at 46800 Jackson Street. The IPD includes three divisions – Support Services Division, Field Services Division, and Investigative Services Division.

### 2.6.2 Fire

The Riverside County Fire Department provides the City of Indio fire protection and emergency services. Four fire stations are located throughout the City to provide timely response and services. Fire Station 86 is the main headquarters of the Fire Administration and Prevention Offices. Fire Station 80 is the nearest station, approximately 2 miles north of the Specific Plan Area.

<b>Riverside County Fire Department Stations</b>	<b>Approximate Distance to Specific Plan Area</b>
Indio Station #1: Station 86 @ 46990 Jackson St.	3.5 Miles
Indio Station #3: Station 88 @ 46621 Madison St.	3 Miles
Indio Station #4: Station 80 @ 81025 Avenue 40	2 Miles
Indio Station 5:	2.5 Miles

Table 2-3 Riverside County Fire Station Locations	
Riverside County Fire Department Stations	Approximate Distance to Specific Plan Area
Station 87 @ 42900 Golf Center Pkwy	

### 2.6.3 Schools

The Specific Plan is served by the Desert Sands Unified School District (DSUSD). Assigned schools include Ronald Reagan Elementary School, Desert Ridge Academy, and Shadow Hills High School.

### 2.6.4 Library Services

Library services are managed by the County of Riverside. The Indio Library, located at 200 Civic Center Mall, City of Indio, is a branch of the County of Riverside library system.

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# Chapter 3 – Development Regulations

## 3.1 General Provisions

This section formally establishes the use restrictions and development standards (zoning) for The Oasis at Indio Specific Plan. The regulations provided herein work in concert with the guidelines set forth in Chapter 4, Design Guidelines, of this Specific Plan to achieve the vision of The Oasis at Indio Specific Plan.

## 3.2 Definition of Terms

- The meaning of words, phrases, titles, and terms shall be the same as provided in the City of Indio Unified Development Code, unless otherwise specifically defined in this Specific Plan.
- **Industrial:** Is intended to accommodate a wide variety of employment-generating uses and protect these areas from incompatible uses. The Industrial Use District (PA 4) provides for offices, business and industrial parks, research and development, warehouses, distribution centers, light manufacturing, public and quasi-public uses, and supportive commercial uses.
- **Mixed-Use:** shall mean a complementary mixture of land uses including commercial, retail, services, office, and residential uses that are part of an integrated single development, either by parcel or within a district. Land uses may be arranged vertically in one or multiple-story buildings or horizontally within separate buildings on a lot, site, or parcel. A mixed-use building is a structure containing residential and commercial uses, including live/work, or two types of commercial uses (retail, services, and office).
- **Multi-Family Residential:** Multi-family attached residential development projects consists of medium to high-density development ranging from 20 du/ac to 50 du/ac. There are a variety of multi-family housing types including duplexes, triplexes, fourplexes, townhomes, and multi-story apartments. The purpose of the provisions in this section is to contribute toward an urban environment of stable and desirable characteristics that is harmonious with existing and future development.
- **Outdoor Dining:** shall mean the extension of services of an existing restaurant or eating/drinking establishment to be provided on the public sidewalk or private common area adjacent to and within the confines of any frontage of that portion of the building where the restaurant use is situated.
- **Pedestrian Zone:** shall mean an area comprised of a pedestrian walkway and planting area with continuous or intermittent trees, shrubs, or planters adjacent to a development parcel.
- **Regular Outdoor Events:** events that have equal amounts of time between them, so that they happen, for example, at the same time each day, week, or month.

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- **Sporadic Events:** shall mean an event that occurs infrequently, irregularly, or occurring in scattered instances.

### 3.3 Performance Standards

#### 3.3.1 Security

Residential units within a mixed-use development shall be designed to ensure residents' security by providing secured entrances and exits that are separate from the non-residential uses and are directly accessible to parking areas. Non-residential and residential uses shall not have common entrance hallways or common balconies. Shared or common residential entrances shall automatically lock upon closing.

#### 3.3.2 Noise and Ventilation

Residential units shall comply with the City's Noise Ordinance, which may include design to limit the interior noise caused by non-residential uses, overhead flights, and/or freeway adjacency to a maximum of forty-five (45) db in any habitable room with windows closed. Proper design may include, but not be limited to, building orientation, sound-rated windows, wall and ceiling insulation, and orientation and insulation of vents. Where windows must be closed to achieve the required level, means shall be provided for ventilation/cooling to provide a habitable environment.

#### 3.3.3 Air Contaminants

Uses, activities, and processes shall not operate in a manner that emits excessive dust, odor, fumes, smoke, or particulate matter unless authorized under Federal, State, or local law. Sources of air emissions shall comply with all rules established by the Environmental Protection Agency (Code of Federal Regulations, Title 40), the California Air Resources Board, and the South Coast Air Quality Management District (SCAQMD). The City shall consult, when appropriate, with SCAQMD to determine which uses shall be equipped with emission-control devices or measures to preclude fugitive dust and particulate emissions from the site. Such devices or measures shall be approved by SCAQMD before issuance of a building permit or other approval authorizing construction activities. The owner shall maintain all devices.

#### 3.3.4 Limitations on Use within a Mixed-Use Development

Any activity or use as determined by the Director not to be compatible with residential uses in a mixed-use development and/or to have the possibility of affecting the health or safety of residents due to the potential for the use to create dust, glare, heat, noise, noxious gases, odor, smoke, traffic, vibration, or other impacts, or create a hazard because of materials, processes, products, or wastes, shall not be permitted within any mixed-use development. This includes but is not limited to storing or shipping of

flammable liquids or hazardous materials beyond what is usually associated with residential use, such as welding, machining, or open flame work.

**3.3.5 Maintenance**

All properties within the Specific Plan Area shall be kept and maintained in a clean, neat, orderly, operable, and usable condition. This requirement applies to buildings, paving, fences, walls, landscaping, and any other structure or feature on the site.

**3.3.6 Hours of Operation**

The hours of operation for commercial/retail uses in a mixed-use development shall be no earlier than 7:00 a.m. and no later than 10:00 p.m. daily unless modified by an administrative or conditional use permit. The hours of operation for industrial warehouse use may operate 24 hours daily.

Construction hours of operation: It shall be unlawful for any person to operate, permit, use, or cause to operate construction equipment outside the hours listed below. However, the Community Development Director and/or City Engineer shall have the authority to allow alternative hours for construction as needed.

Construction Hours	Pacific Standard Time	Pacific Daylight Time
Monday through Friday	7:00 a.m. through 6:00 p.m.	6:00 a.m. through 6:00 p.m.
Saturday	8:00 a.m. through 6:00 p.m.	7:00 a.m. through 6:00 p.m.
Sunday and Government Holidays	9:00 a.m. through 5:00 p.m.	9:00 a.m. through 5:00 p.m.

**3.3.7 Loading**

Loading, unloading, and all service and maintenance activities shall be conducted within the hours of operation noted in Subsection 3.3.6 *Hours of Operation*.

**3.3.8 Lighting**

Lighting for nonresidential uses shall be appropriately designed, located, and shielded to ensure that it does not negatively impact the residential units in the development or any adjacent residential uses and shall comply with Indio Unified Development Code (IUDC) Section 3.02.11 *Outdoor Lighting*.

**3.4 Permitted Uses**

This section identifies the permitted, conditional, and accessory uses within the Specific Plan Area. Uses in the table are permitted subject to the permit criteria referenced. The Land Use Districts identified in Figure 2-1 provide a visual representation of the desired location of various potential uses for evaluation by the City during the review of development proposals. Three land use districts are included: Residential, Commercial/Retail, and Industrial. The residential and commercial/retail land uses allow for

### Chapter 3 Development Regulations

both horizontal and vertical mixed-use on individual parcels and stand-alone uses allowed by Table 3-1, *Land Use Matrix*, below. Uses identified by:

- “P” is permitted by right in the designated land use district, subject to compliance with all applicable provisions of The Oasis at Indio Specific Plan development standards.
- “AUP” is permitted in the designated land use district following consideration by the Director to ensure that the use can be designed, located, and operated in a manner that will not interfere with the use and enjoyment of surrounding properties. An AUP is subject to the filing of an Administrative Use Permit in accordance with the requirements of the City of Indio Unified Development Code, Section 6.04.04 *Use Permits*.
- “CUP” is permitted in the designated land use district following consideration by the Planning Commission to ensure that the use can be designed, located, and operated in a manner that will not interfere with the use and enjoyment of surrounding properties. A CUP is subject to filing a Conditional Use Permit per the requirements of the City of Indio Unified Development Code, Section 6.04.04 *Use Permits*.
- “TUP” is used for an event, activity, or use on a short-term basis. Such use is permitted in conjunction with filing a Temporary Use Permit per the requirements of the City of Indio Unified Development Code, Section 6.04.05 *Temporary Use Permits*.
- “A” is permitted by right as an ancillary use in conjunction with the property’s primary permitted or conditionally permitted use.
- “–” is not allowed within the designated land use district.

Any use not included shall be considered prohibited unless deemed to be similar to an allowable use as interpreted by the Community Development Director or his/her designee.

Where there are special development standards or regulations, the Indio Unified Development Code (IUDC) section is identified. Planning Review Applications shall be processed in accordance with Chapter 5 herein.

Table 3-1 Land Use Matrix			
P = Permitted; AUP = Administrative Use Permit; CUP = Conditional Use Permit; TUP = Temporary Use Permit; A = Ancillary Use; -- = Not Permitted			
Use <sup>1</sup>	Land Use District		Additional Notes
	Mixed-Use	Industrial	
<b>Agriculture and Animal Keeping Uses</b>			
Agricultural Support, Sales, Service, and Storage	--	P	
Aquaculture	--	AUP	

<b>Table 3-1 Land Use Matrix</b>			
P = Permitted; AUP = Administrative Use Permit; CUP = Conditional Use Permit; TUP = Temporary Use Permit; A = Ancillary Use; -- = Not Permitted			
<b>Use<sup>1</sup></b>	<b>Land Use District</b>		<b>Additional Notes</b>
	<b>Mixed-Use</b>	<b>Industrial</b>	
Animal Raising and Keeping, Domestic Pets	A	--	See IUDC Ch. 4.04 (Animal Keeping)
Community Garden	A	A	
Greenhouse, Private	P	P	
Private Garden	A	A	
<b>Automobile and Vehicle Uses</b>			
Auto and Vehicle Rental	P	--	See IUDC Ch. 4.05 (Automotive Service and Repair/Fueling Stations)
Auto and Vehicle Sales, New	P	--	
Auto and Vehicle Sales, Used	P	--	
Auto and Vehicle Sales and Rental, Large Vehicles and Equipment	--	AUP	
Auto and Vehicle Services and Repair – Major	--	P	See IUDC Ch. 4.05 (Automotive Service and Repair/Fueling Stations)
Auto and Vehicle Services and Repair – Minor	P	P	See IUDC Ch. 4.05 (Automotive Service and Repair/Fueling Stations)
Auto and Vehicle Washing and Detailing	P	A	
Fueling Stations	AUP	AUP	See IUDC Ch. 4.05 (Automotive Service and Repair/Fueling Stations)
Truck Stop/Fueling Station	AUP	AUP	See IUDC Ch. 4.05 (Automotive Service and Repair/Fueling Stations)
<b>Cannabis-Related Uses</b>			
Cannabis Cultivation, Personal	P	--	Personal indoor cultivation of 6 or fewer live cannabis plants within a single private residence or inside an accessory structure located upon the grounds of a

Table 3-1 Land Use Matrix			
P = Permitted; AUP = Administrative Use Permit; CUP = Conditional Use Permit; TUP = Temporary Use Permit; A = Ancillary Use; -- = Not Permitted			
Use <sup>1</sup>	Land Use District		Additional Notes
	Mixed-Use	Industrial	
			private residence that is a fully enclosed and a secure structure is permitted per Chapter 4.07 (Cannabis Activity). Outdoor personal cultivation of live cannabis plants is prohibited.
Cannabis Retailer, Microbusiness, or Dispensary	P	--	See Chapter 4.06 (Cannabis Activity)
<b>Eating and Drinking Establishments and Entertainment Uses</b>			
Bar/Nightclub/Lounge	P	P	
Brewpub/Taproom/Wine Bar/Micro distillery	P	P	
Cinema/Theater/Performing Arts Center	P	P	
Drive-Through, Non-Restaurants (Accessory)	A	--	
Live Entertainment as Accessory Use, Indoors (Accessory)	A	A	See IUDC Section 4.27.02 (Temporary Uses)
Live Entertainment as Accessory Use, Outdoors (Accessory)	A, AUP, TUP	A, AUP, TUP	Regular outdoor events as an accessory use require an AUP. Sporadic events require a TUP (See IUDC Chapter 4.27 (Temporary Uses).
Outdoor Dining (Accessory)	A	A	
Restaurant, Dine-In and Take-Out	P	P	
Restaurant, Drive-Through	P	--	
<b>Industrial and Manufacturing Uses</b>			
Artisan Manufacturing/Makers Space	AUP	P	
Brewery/Distillery/Winery - without tasting room or > 10,000 sf	--	P	
Brewery/Distillery/Winery - with tasting room and <10,000 sf	AUP	P	

<b>Table 3-1 Land Use Matrix</b>			
P = Permitted; AUP = Administrative Use Permit; CUP = Conditional Use Permit; TUP = Temporary Use Permit; A = Ancillary Use; -- = Not Permitted			
Use <sup>1</sup>	Land Use District		Additional Notes
	Mixed-Use	Industrial	
Construction and Materials Yards	--	AUP	
Food or Beverage Manufacturing	--	AUP	
Indoor Warehousing, Storage, Wholesaling, and Distribution	--	P	
Manufacturing/Processing, Light	--	P	
Outdoor Storage (Primary Use)	--	AUP	See IUDC Ch. 4.20 (Outdoor Storage)
Outdoor Storage (Accessory)	A	A	See IUDC Ch. 4.20 (Outdoor Storage)
<b>Lodging</b>			
Bed and Breakfast	CUP	--	
Lodging – Hotels/Motels/Extended Stay	P	--	
<b>Recreation, Education, and Public Assembly Uses</b>			
Instructional Services, <= 5000 sf	P	P	
Instructional Services, > 5000 sf	P	P	
<b>Residential Uses</b>			
Accessory Dwelling Unit (ADU) & Junior Accessory Dwelling Unit (JADU)	A	--	Per IUDC Ch 4.02 (Accessory Dwelling Units)
Child Day Care in a Home, Small	P	--	See IUDC Ch. 4.09 (Child Day Care in a Home (Small and Large) and Day Care Centers)
Home Occupation	A	--	See IUDC Ch. 4.15 (Home Occupations)
Cottage Food Operation	A/AUP	--	See IUDC Ch. 4.11 (Cottage Food Operations)
Live/Work	P	--	See IUDC Ch. 4.16 (Live/Work)
Residential Care Facilities, Large	CUP	--	
Residential Care Facilities, Small	P	--	
Multi-Family Dwelling, Triplex/Quadplex	P	--	

Table 3-1 Land Use Matrix			
P = Permitted; AUP = Administrative Use Permit; CUP = Conditional Use Permit; TUP = Temporary Use Permit; A = Ancillary Use; -- = Not Permitted			
Use <sup>1</sup>	Land Use District		Additional Notes
	Mixed-Use	Industrial	
Multi-Family Dwelling, ≥ 5 Units	P	--	
Single-Room Occupancy	CUP	--	
Small-Lot Detached Single-family	P		See IUDC Section 2.03.04
Supportive Housing	CUP	--	
Transitional Housing	CUP	--	
Yard Sales (Temporary)	P	--	See IUDC Section 4.27.02 (Temporary Uses)
<b>Retail, Service, and Office Uses</b>			
Alcohol Sales, Off-Sale	P	P	
Animal Sales and Services	P	--	
ATM (Accessory Use)	A	--	
Banks, Financial, and Savings and Loan Institutions	P	--	
Building Materials Sales and Services	AUP	P	
Business Support Services	AUP	P	
Convenience Market	P	--	
Food Preparation, Commercial	P	P	
Fortunetelling, Palm and Card Reading	P	--	
Funeral Homes and Mortuaries	P	P	
General Retail ≤ 5,000 sf	P	P	
General Retail/Superstore > 25,000	P	P	
Grocery Store	P	P	
Kennels/Boarding, Commercial	P	P	
Kiosk/Outdoor Vending	P	--	
Laundromat	P	--	
Massage Establishment, Accessory Use	A	--	
Massage Establishment, Stand-Alone	CUP	--	See IUDC Ch. 4.18 (Massage Establishment (Stand-alone))

<b>Table 3-1 Land Use Matrix</b>			
P = Permitted; AUP = Administrative Use Permit; CUP = Conditional Use Permit; TUP = Temporary Use Permit; A = Ancillary Use; -- = Not Permitted			
Use <sup>1</sup>	Land Use District		Additional Notes
	Mixed-Use	Industrial	
Medical Services, Extended Care	AUP	AUP	
Medical Services, Medical/Dental/Holistic/Clinic	P	P	
Medical Services, Urgent Care	P	P	
Mini-Storage Warehousing or Facility	--	AUP	See IUDC Ch. 4.19 (Mini-Storage Warehousing or Facility)
Neighborhood Market	P	P	
Nursery/Garden Center	P	P	
Offices, Processing	P	P	
Offices, Professional /Administrative	P	P	
Offices, Service	P	P	
Outdoor Display (Accessory)	A	A	
Pawnshop	CUP	--	See IUDC Ch. 4.21 (Pawnshops)
Personal Services	P	P	
Research and Development	AUP	P	
Christmas tree sales, sidewalk and parking lot sales, seasonal sales, etc.	P	P	See IUDC Section 4.27.02 (Temporary Uses)
Smoke Shops	P	--	See IUDC Ch. 4.24 (Smoke Shops)
Smoking Lounge	P	--	
Tattoo/Body Art/Piercing	P	--	See IUDC Ch. 4.25 (Tattoo/Body Art/ Piercing Establishment)
Resale/Consignment/Thrift Shop	P	--	See IUDC Ch. 4.23 (Resale/ Consignment/ Thrift Shops)
Veterinary Hospitals	P	P	
<b>Utility, Transportation, and Communication Uses</b>			
Communications, Facilities within Buildings	AUP	P	

**Chapter 3 Development Regulations**

<b>Table 3-1 Land Use Matrix</b>			
P = Permitted; AUP = Administrative Use Permit; CUP = Conditional Use Permit; TUP = Temporary Use Permit; A = Ancillary Use; -- = Not Permitted			
<b>Use<sup>1</sup></b>	<b>Land Use District</b>		<b>Additional Notes</b>
	<b>Mixed-Use</b>	<b>Industrial</b>	
Communications, Telecommunications / Large Commercial Antennas	AUP/CUP	AUP/CUP	An AUP or CUP is required, depending on the design and height of the large commercial antenna, as regulated by Section 4.26.03 (Large Commercial Antennas). See IUDC Ch. 4.26 (Telecommunications/ Wireless Facilities)
Communications, Telecommunications / Small Wireless Facilities	P	P	See IUDC Ch. 4.26 (Telecommunications/ Wireless Facilities)
Freight / Trucking Facility	--	AUP	
Light Fleet-Based Services	AUP	P	
Mobile Recycling Unit	A	A	
Parking Lots and Structures	A/AUP	P	
Renewable Energy System (Primary Use)	CUP	P	
Renewable Energy System, Building-Mounted (Accessory)	A	A	
Renewable Energy System, Stand-Alone or Ground Mounted (Accessory)	A, AUP	A	
Reverse Vending Machine (Accessory)	A	A	

Notes:

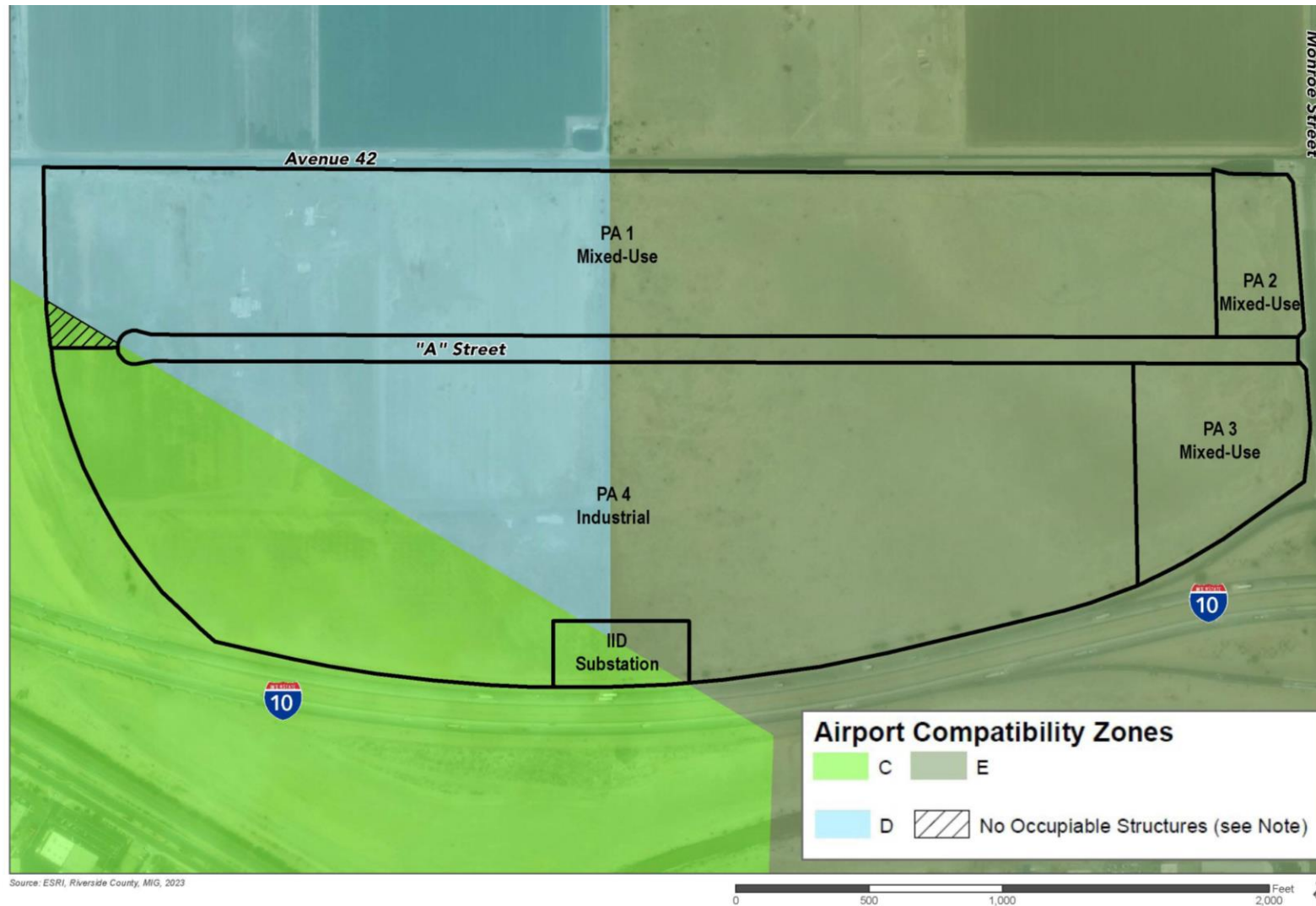
1. Uses shall be consistent with the criteria of the Bermuda Dunes Airport Compatibility Plan, including Table 3-2, Maximum Density/Intensity Calculations, and applicable County-wide criteria as may exist at the time of project review. Where the project is located in the Bermuda Dunes Airport Influence Areas, compatibility zones and regulations apply. See Appendix A.

Table 3-2 Maximum Density / Intensity Calculations				
Residential Densities	ALUC Zone			
<ul style="list-style-type: none"> <li>• DU/AC<sup>1</sup></li> </ul>	<p><b>Zone C:</b> 0.2 (average parcel size ≥5.0 ac.)</p> <p><b>Zone D:</b> Two options are provided for residential densities in Compatibility Zone D. Option (1) has a density limit of 0.2 dwelling units per acre (i.e., an average parcel size of at least 5.0 gross acres). Option (2) requires that the density be greater than 5.0 dwelling units per acre (i.e., an average parcel size less than 0.2 gross acres). The choice between these two options is at the discretion of the local land use jurisdiction. See Table 2B for an explanation of the rationale. All other criteria for Zone D apply to both options.</p> <p><b>Zone E:</b> No Limit</p>			
Non-Residential Intensities	ALUC Zone			Notes
	C	D	E <sup>2</sup>	
<ul style="list-style-type: none"> <li>• Average</li> </ul>	75	100	No Limit	The total number of people permitted on a project site at any time, except rare special events, must not exceed the indicated usage intensity times the gross acreage of the site. Rare special events are ones (such as an air show at the airport) for which a facility is not designed and normally not used and for which extra safety precautions can be taken as appropriate.
<ul style="list-style-type: none"> <li>• Single Acre</li> </ul>	150	300		Clustering of non-residential development is permitted. However, no single acre of a project site shall exceed the indicated number of people per acre. See Policy 4.2.5 for details.
<ul style="list-style-type: none"> <li>• With Bonus</li> </ul>	198	390		An intensity bonus may be allowed if the building design includes features intended to reduce risks to occupants in the event of an aircraft collision with the building. See Policy 4.2.6 for details.
Required Open Land	20%	10%	No Req't	Open land requirements are intended to be applied with respect to an entire zone. This is typically accomplished as part of a community general plan or a specific plan but may also apply to large (10 acres or more) development projects. See Policy 4.2.4 for the definition of open land.

Notes:

1. Usage intensity calculations shall include all people (e.g., employees, customers/visitors, etc.) who may be on the property at a single point in time, whether indoors or outside.
2. Although no explicit upper limit on usage intensity is defined for Zone E, land uses of the types listed—uses that attract very high concentrations of people in confined areas—are discouraged in locations below or near the principal arrival and departure flight tracks. This limitation notwithstanding, no use shall be prohibited in Zone E if its usage intensity is such that it would be permitted in Zone D.

Figure 3-1: Conceptual Site Plan with Airport Compatibility Zones



Source: ESRI, Riverside County, MIG, 2023

Note: In the PA1 area is an approximately 0.5 acre area that is hashed out. The PA1 area can be developed with structures of 20-50 du/ac, however, this portion of the PA1 area is also in the Airport land use Zone C which restricts residential density to a maximum of 0.2 du/ac. Therefore this area shall not be developed with occupiable structures but can be used for parking, open area, or other non-structure development.

### 3.5 Development Standards

The following standards establish the development criteria that shall apply within The Oasis at Indio Specific Plan Area

Table 3-3 Development Standards <sup>1, 2 &amp; 3</sup>					
Standard	Mixed-Use District			Industrial	Additional Comments/Regulations
	MF Residential	MU	COM		
<b>Lot and Density Standards</b>					
Density (Units/Gross Acres)	20-50		--		See Indio Municipal Code Chapter 154 (Density Bonuses and Other Incentives for Low-Income and Senior Housing).
Min. Project Site	10,000 sf				
Min. Lot Width/Depth	N/A	N/A		N/A	
Max. Non-Residential Floor Area Ratio <sup>4</sup>		0.50		0.60	
Max. Height (ft)/Stories	60 ft/4 stories	60 ft/4 stories	60 ft/3 stories	Zone C: 70 ft Zone D/E: 100 ft	Subject to the Bermuda Dunes Airport Land Use Compatibility Plan. FAA review would be required for any structures with top of roof exceeding 127 feet above mean sea level (AMSL).
Max. Height (ft) – w Architectural Features	70 ft			---	
Max. Height (ft) - Accessory Building	30 ft	---	---	---	
Min. Ground Floor Height –	N/A	18 ft	---	---	

Table 3-3 Development Standards <sup>1, 2 &amp; 3</sup>					
Standard	Mixed-Use District			Industrial	Additional Comments/Regulations
	MF Residential	MU	COM		
Non-residential (Floor to Floor)					
<b>Minimum/Maximum Building Site Setback<sup>5</sup></b>					
Avenue 42	25 ft	10 ft/20 ft	10 ft	---	See IUDC Section 2.03.06 Mixed-Use & M-F Residential Design Standards, Figure 2.03.06-1 ( <i>Building Frontage Location</i> )
Monroe Street	10 ft	10 ft	10 ft	---	
“A” Street <sup>6</sup>	50ft	15 ft	15 ft	10 ft	Muni Code: Per Section 95C.08 (A) Standing motor vehicles. No person shall operate or permit the operation of any motor vehicle with a gross vehicle weight rating in excess of 10,000 pounds, or any auxiliary equipment attached to such a vehicle, including but not limited to refrigerated truck compressors, for a period longer than 15 minutes in any hour while the vehicle is stationary and on a public right-of-way or public space, within 150 feet of a residential area between the hours of 7:00 p.m. and 7:00 a.m., except when movement of said vehicle is restricted by other traffic.
Private Street	5 ft	5 ft	5 ft		

Table 3-3 Development Standards <sup>1, 2 &amp; 3</sup>					
Standard	Mixed-Use District			Industrial	Additional Comments/Regulations
	MF Residential	MU	COM		
Alley or Common Drive	5 ft	5 ft	5 ft	--	
Interior Property Line	5 ft	10 ft	10 ft	0 ft; 20 ft adjacent to residential	
Freeway Right-of-Way	20 ft	20 ft	10 ft	10 ft	
Distance between Structures	Minimum required by adopted Building and Fire Code.				
Parking Frontage <sup>7</sup>					
Max. Surface Parking Along Avenue 42 and Monroe St. Frontage	30%	30%	---	---	No more than 30% of the primary street frontage shall be devoted to parking. (See IUDC Figure 2.03.03-1. Maximum Parking Frontage Along a Primary Street)

Table 3-3 Development Standards <sup>1, 2 &amp; 3</sup>					
Standard	Mixed-Use District			Industrial	Additional Comments/Regulations
	MF Residential	MU	COM		
<b>Minimum Open Space Standards</b>					
Minimum Per Unit	190sf/unit; Min. of 100 sf of private open space is required for ground-level units and 60 sf of private open space for upper-level units.	100 sf/unit; Mixed-Use Developments; a minimum of 50% of the units must have 60 sf of private open space.	---	---	<p>Minimum private open space is 6 feet in either direction.</p> <p>Common open space includes roof decks and other shared common open areas.</p> <p>Minimum common open space dimension is 20 feet in either direction. Setback areas are not considered usable open space unless they are at least 20 feet wide.</p> <p>Up to 40 sf per unit can be provided in an indoor communal space so long as it opens directly onto a common outdoor space.</p>
Min. Usable Open Space applies to mixed-use Commercial over 25K sf	---	3% of gross floor area	---	---	<p>Could be common open space for employees or publicly accessible open space. If the open space is over 1,000 square feet and provided as publicly accessible, up to 50 percent may be counted towards the required residential common open space.</p>
Min. Landscaping (% of lot)	10% of the lot, of which 50% must be planted with live	10% of the lot, of which 25% must be live plant material.			

Table 3-3 Development Standards <sup>1, 2 &amp; 3</sup>					
Standard	Mixed-Use District			Industrial	Additional Comments/Regulations
	MF Residential	MU	COM		
	plant material				
Minimum Parking Lot Landscaping	15% of the total off-street open parking area shall be Landscaped. A min. 1/3 of the required landscaping shall be distributed within the interior of the parking facility and the remaining two-thirds shall be provided as peripheral planting on the exterior edges of the parking area.				
<b>Other Standards</b>					
Rules of Measurement				See IUDC Chapter 3.01	
Accessory Structures				See IUDC Section 3.02.04	
Setbacks and Encroachments				See IUDC Section 3.02.03	
Utilities, Services Areas, and Building Equipment				See IUDC Section 3.02.07	
Refuse and Recycling				See IUDC Section 3.02.08	
Landscaping				See IUDC Section 3.02.09	
Fences, Walls, and Screening				See IUDC Section 3.02.10	
Outdoor Lighting				See IUDC Section 3.02.11	
Off-Street Loading				See IUDC Section 3.03.10	
Bicycle Parking				See IUDC Section 3.03.09	
Signs				See IUDC Chapter 3.05	

## Notes:

- Properties located within the Airport Influence Area (AIA) shall be subject to the requirements and standards of the Bermuda Dunes Airport Compatibility Plan (ALUCP).
- Densities and intensities shall be consistent with the criteria of the Bermuda Dunes Airport Compatibility Plan, including applicable County-wide criteria as may exist at the time of project review.
- Number of employees shall be based on the largest shift that occurs in a typical week.
- Individual parcels within PA 4 may exceed the maximum FAR provided that the sum of the industrial development FAR within PA 4 does not.
- Setbacks from public streets shall be measured from public right-of-way. "A" Street setback shall be measured from lot line/right-of-way.
- A 50-foot-wide minimum setback along the north side of "A" Street in PA 1 shall include an enhanced landscaped transition zone. The landscape shall encompass a living desert landscape environment comprised of large canopied evergreen shade trees, shrubs, and semi-tropical flowering plants to create a natural lush low-water landscape with a variety of shapes, intriguing color palettes, and textures. Landscaping may include walkways, benches, trellises, thematic fencing and walls, and related amenities.
- Surface parking limitations are not applicable along "A" Street.

### 3.5.1 Supplemental Development Standards

1. Certain real estate transactions involving residential property within any compatibility zone must disclose information regarding airport proximity and the existence of aircraft overflights. California state statutes set this requirement (Business and Professional Code Section 11010 and Civil Code Sections 1102.6, 1103.4, and 1353). With certain exceptions, these state requirements apply both to the sale or lease of newly subdivided lands and to the sale of existing residential property.
2. Deed Notices. A deed notice shall be recorded for each parcel associated with any discretionary land use action affecting property within an airport influence area. The notice shall include the language indicated below with respect to real estate transfer disclosures.

**NOTICE OF AIRPORT IN VICINITY:** This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.

3. Access - Appropriate access routes necessary for fire and safety access, subject to City approval, shall be provided for all areas of a project. See also the City standards in the City Design and Engineering Manual.
4. All developments shall use desert-appropriate sustainable materials that can withstand the desert environment.

#### 3.5.1.1 Supplemental Multiple-Family and Mixed-Use Development Standards (PA 1, PA 2, and PA 3)

1. Table 3-3 Development Standards (Minimum Open Space Requirements for Multiple-Family and Mixed-Use Development Projects), above, establishes the minimum open space requirements for multiple-family development projects and the multiple-family residential portion of mixed-use development projects. The required open space area shall be calculated on a per unit basis and includes [i] private open space for the exclusive use of a dwelling's occupants and guests, and [ii] common open space areas for the enjoyment of all residents within a development project. Common open space consists of active areas, with recreation facilities, and passive areas incorporating features that enhance the appearance and desirability of a development project including plantings, pathways, waterscape, hardscape, rockscapes, shade structures, seating, raised planters, and other unique features.
2. The ratios of "private" open space area to "common" open space area specified in Table 3-3 Development Standards (Minimum Open Space Requirements for Multiple-Family and Mixed-Use Development Projects), above, are recommended and may be adjusted by the developer, based upon the housing market the proposed development is intended to serve, and subject to

approval by the approving authority for the project. At a minimum, the “required total” open space for multiple-family development projects shall be provided.

3. Publicly Accessible Open Space (PAPOS) and private common open space in multi-family residential projects and residential mixed-use projects shall comply with Article 2, Section 2.03.06.I of the IUDC.
4. Private Open Space is intended for private use for each dwelling unit and may include balconies (covered or uncovered), private gardens, private yards, terraces, decks, and porches, among others. Private open spaces shall meet the following standards:
  - a) Be directly accessible from a residential unit.
  - b) Minimum dimension of 6 feet in any direction. Entry porches with a minimum dimension of 6 feet by 10 feet.
  - c) Minimum clear height dimension of 8 feet 6 inches.
  - d) May be covered but cannot be enclosed. Private open space is considered enclosed when the space between a floor, decking, or ground level and a roof structure has more than two sides taller than 50 inches in height.
  - e) Ground level private open space shall be screened or buffered from adjacent private or common open space and dwellings by landscaping, fencing, walls, trellises, or other screening elements. Such screening or buffering shall be a minimum of 30 inches and a maximum of 42 inches in height.
5. Residential units shall be designed to ensure residents’ security by providing separate and secured entrances and exits that are directly accessible to parking areas. Where residential units are in the same structure as non-residential/commercial uses, access to residential units shall be from a secured area or entrance located on the ground floor. Nonresidential and residential uses located on the same floor shall not have common entrance hallways or common balconies. Shared or common residential entrances shall automatically lock upon closing.
6. A mixed-use building containing two types of commercial uses (retail, services, and office), can collectively be on all floor levels.
7. Subterranean parking may extend to all property lines.
8. Block Size - The maximum block size shall be no greater than four acres in size. No block shall be greater than 800 feet in length without a publicly accessible path or street connecting from one public right-of-way to another public right-of-way or turnaround point. Alleys can count as a connecting street or publicly accessible path.
9. External Connectivity - Streets within any proposed building/development site shall, to the extent feasible and unless alternatively approved by the Director, be aligned with existing and planned streets and pathways in the surrounding area to create a continuous street pattern. All

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streets, alleys, pedestrian and bike pathways in any building/development site shall connect to other streets and to existing and planned public sidewalks, streets, open spaces, and bike paths outside the proposed development to the extent feasible. Any dead-end street longer than 300 feet shall be connected to other streets by a pedestrian path.

10. Multi-Modal Site Access - Pedestrian and bike connections shall connect to existing and proposed public sidewalks, streets, open spaces, bike paths, bicycle parking areas, and automobile parking areas adjacent to the Specific Plan Area.
11. A pedestrian pathway or multi-use path (pedestrian and/or bicycle) between buildings or through parking lots from the sidewalk to the interior of the site shall be provided for every 800 feet of a project's frontage.
  - a) Pedestrian pathways/sidewalks shall have a minimum six-foot wide unimpeded throughway for the extent of the path or sidewalk.
  - b) Multi-use paths through sites that connect two parallel public or publicly accessible rights-of-way shall permit at a minimum one location with 24-hour access for pedestrians, cyclists, and as appropriate emergency vehicles, and shall provide at minimum a 10-foot-wide path.
12. Pedestrian pathways/sidewalks connecting to the public sidewalk and other planned or existing pedestrian routes or trails shall be provided. A pedestrian pathway/sidewalk shall connect the primary building entry or entries to the public sidewalk on each street frontage. Pedestrian pathways/sidewalks a minimum of five feet wide shall be provided from the public right-of-way to all primary entryways, common areas, guest/customer vehicle parking, and bicycle parking areas.
13. Where a non-residential zoned property abuts a residential or mixed-use zoned property, a solid masonry wall shall be installed along the adjacent interior property lines with a minimum height of six feet and a maximum height of eight feet.

### 3.5.1.2 Drive-through Aisle Standards

1. The entrance/exit of any drive aisle shall be a minimum of 50 feet from an intersection of public rights of way (measured at the closest intersecting curbs).
2. A clearly identified area shall be provided for vehicles waiting for drive-up or drive-through service that is physically separated from other onsite traffic circulation. The Director may approve alternatives to the requirements of this Subsection where it is determined that the alternate design will, given the characteristics of the site, be equally effective in ensuring on- and off-site pedestrian and vehicular traffic safety and minimizing traffic congestion.
3. The stacking area shall accommodate a minimum of five cars for each drive-up or drive-through window in addition to the vehicle receiving service.
4. The stacking area shall be located at and before the service window (e.g., pharmacy, teller, etc.).

5. Separation of the stacking area from other traffic shall be done by concrete curbing or paint striping on at least one side of the lane.
6. The drive-through aisle shall be screened from the sidewalk or street with landscaping or decorative walls and berms.

### 3.5.1.3 Supplemental Industrial Development Standards (PA 4)

1. A Fourteen-foot-wide landscaped area (including four feet of right-of-way) shall be provided along "A" Street.
2. A pedestrian pathway between buildings or through parking lots from the sidewalk to the interior of the site shall be provided at the primary ingress/egress access point of a project's frontage.
  - a) Pedestrian pathways shall have a minimum six-foot wide unimpeded throughway for the extent of the path.
  - b) A five-foot wide pedestrian pathway shall connect the primary building(s) entry to the public sidewalk on the south of "A" Street.
3. Differentiated paving materials, textures (i.e., stamped patterns), and/or colors shall be required where pedestrian paths cross parking areas (where applicable).
4. Loading docks shall be permitted on building facades that directly face "A" Street.
5. Loading docks and truck parking areas shall be visually screened from view along the internal "A" Street with a 12-foot-high wall, landscaping, and/or other screening features or barriers (such as berms).
6. Gated entry points are permitted in Planning Area 4. If entry gates are used, they shall be positioned to allow enough distance for the stacking of at least two (2) trucks on the lot to preclude queuing of trucks on public streets.
7. Outdoor storage may not be located within any required front or street-side yard. Outdoor storage areas shall be screened from view.
8. Outdoor loading and storage areas and loading doors shall be screened from public view by decorative concrete or masonry walls with lockable view obstructing gates. Such walls shall be a minimum of eight feet in height and shall be of sufficient height to screen all outdoor materials and equipment, tractors, trailers, and loading doors from public view.
9. For security purposes, fencing along the I-10 may include fencing or wall material at least six feet in height.
10. Semi-truck idling shall be restricted to fifteen minutes.

## Chapter 3 Development Regulations

11. The loading and unloading of vehicles, operating of forklifts or cranes shall be prohibited within 200 feet of a residence.

### 3.6 Parking

Parking standards for uses within the Specific Plan Area are shown in Table 3-4: Off-Street Vehicle Parking Standards.

Table 3-4 Off-Street Vehicle Parking Standards	
Use	Standards
<b>Residential</b>	
Multi-Family Dwellings	<ul style="list-style-type: none"> <li>• 1 per studio or one-bedroom unit</li> <li>• 1.5 per two-bedroom unit</li> <li>• 2 per unit with three or more bedrooms</li> <li>• Plus 0.2 guest spaces per unit</li> </ul>
Live/Work	1 space per 1,000 square feet of nonresidential use 1 space per residential unit.
<b>Commercial</b>	
Commercial, Inclusive of Retail, office Use, Services, Restaurants (Dine-In, Take-Out and Drive-Thru)	3.5 spaces/1,000 sf
Lodging – Hotels, Motels, Extended Stay, Inclusive of ancillary retail, food service, and conference	1.25 spaces/key
Community Assembly	1 space/4 fixed seats, or 1 space/40 sf of assembly area if no fixed seats
<b>Industrial</b>	
Freight/Trucking Facility	2 spaces/1,000 sf of office space, plus 1 space/service or fleet vehicle, plus 1 space/service or fleet vehicle. Trailer parking: 1 trailer parking space per loading door.
Warehouse	Up to 200,000 SF: 1 space per 1,250 sf of gross floor area All Additional SF: 1 space per 5,000 sf of gross floor.
	Trailer parking: 1 trailer parking space per loading door.
Ancillary Office	1 space/1,000 sf
Manufacturing/Processing, Light	0.5 spaces/1,000 sf

In addition to the ratios shown, the following criteria are required:

1. Required parking spaces must be available for residents, customers, or employees of the use. Parking and loading areas shall be accessible for their intended purpose during all hours of operation. Fees may be charged for the use of required parking spaces.
2. Stacked or valet parking is allowed if an attendant is present, or an automated system is in place to move vehicles. Uses with valet parking shall provide an approved valet parking plan which includes insurance provisions for patrons. Such provisions must be approved by the City Attorney. If stacked parking managed by an attendant is used for required parking spaces, an acceptable form of guarantee must be filed with the Director ensuring that an attendant will be present while the parking lot is in operation.
3. Residential parking spaces in a mixed-use development shall be assigned to residential occupants.
4. Parking ratios and fraction calculations shall be governed by IUDC 3.03.05.E (Calculation of Required Spaces).
5. An Ancillary use in conjunction with the primary use on the property shall not be subject to additional parking.
6. In a mixed-use building where ancillary service or small-scale food service commercial uses are a small component of the project (less than or equal to 2,000 square feet or 20% of the first-floor area, whichever is more stringent), no additional parking is required. Commercial uses more than 2,000 square feet or over 20% of the floor area shall be parked per the ratios in Table 3-4.
7. Exceptions and reductions to off-street parking shall be governed by IUDC 3.03.06 (Exceptions and Reductions to Off-Street Parking).
8. Required parking for residential uses shall be on the same lot as the dwelling or use they serve on a parcel or tract owned in common by all the owners of the properties that will use the parking area. Required parking spaces serving nonresidential uses shall be located on the same lot as the use they serve.
9. Parking Development and Design Standards shall be governed by IUDC 3.03.08 (Parking Development and Design Standards).
10. In the event that parking characteristics evolve due to new technologies or changes in parking demand, modified parking standards may be proposed based upon a parking demand analysis reviewed and approved by the City as part of the development plan review.
11. New technologies such as parking lifts are permitted, subject to review as part of the development plan review.
12. Handicapped parking spaces shall be provided in accordance with State and Federal regulations and shall be considered in the calculation of required spaces.

### Chapter 3 Development Regulations

13. Electric Vehicle (EV) charging stations shall be governed by Title 24 requirements at the time of permit issuance.
14. Bicycle parking shall be governed by IUDC 3.03.09 (Bicycle Parking) based on use type.

# Chapter 4—Design Guidelines

The general framework for the physical design of The Oasis at Indio Specific Plan is to ensure the development of a high-quality and cohesive mixed-use project while allowing for variation and flexibility. These guidelines are intended to complement the Development Standards outlined in Chapter 3 and to provide general directions on implementing the vision and fundamental concepts established by the Specific Plan for the future design of buildings, streetscapes, and other spaces. These guidelines are not a set of rigid requirements; instead, they are general and illustrative in nature and are intended to provide a range of flexibility to encourage creativity and variety on the part of designers. The overall goal is to ensure that the intent and spirit of the design guidelines are followed to attain the best possible design solutions.

The objectives of these Design Guidelines are to:

- Promote compatibility with surrounding uses.
- Ensure a site design that provides appropriate transitions and buffer space and/or design features that reduce negative impacts at the source.
- Provide guidelines that allow flexibility for practical application and creative expression.
- Ensure functional pedestrian and motor vehicle circulation within a project and convenient pedestrian connections between residential and retail commercial developments.
- Provide landscaping that improves the streetscape experience and is supportive of the site’s character and the character of the surrounding community.

The Oasis at Indio Specific Plan features a contemporary mixed-use development containing multi-family residential, commercial, and industrial uses that take advantage of the property’s location near regional transportation corridors. The Oasis at Indio is a place where businesses can locate, thrive, and have access to goods and services, as well as where multi-family homes in an urban residential area are within walking distance -of shopping, employment opportunities, and a variety of transportation modes. Many of the design guidelines apply to all Planning Areas within the Specific Plan Area. However, due to the different land use districts and settings, some guidelines specifically apply to residential, commercial, mixed-use, or industrial land uses. The applicable planning area is referenced to establish which guideline applies. These Guidelines will be implemented through careful coordination between the master developer and the City of Indio during the entitlement review process.

## 4.1 Multi-Family Guidelines

### *Site Design*

- Ensure that new residential developments provide adequate on-site recreational and open space amenities, generous streetscapes, and landscaped front yards consistent with the values and standards of the community and the needs of new development.
- Buildings should be oriented to promote privacy for individual residential units to the greatest extent possible.

## Chapter 4 Design Guidelines

- Buildings should be grouped to create a functional and visual relationship between them and clustered when feasible to maximize surface/ground level areas for other uses such as parks, usable open spaces, and paseos.
- Establish connections among buildings through structural and landscape elements.
- Provide an array of pedestrian amenities integrated into the development's overall design and character, such as seating areas, drinking fountains, landscape planters, water fountains, shade canopies, and trash receptacles.
- Buildings should be oriented towards public spaces or common areas to provide “eyes on the street” to create welcoming and safe places for residents.



*Ex: Simple geometric forms, repeated patterns, mass breaks, and building articulation.*

### Parking

- Structured, surface, and semi-subterranean parking areas should be designed and placed to be unobtrusive and have minimal impact on the character and scale of residential and mixed-use land uses.
- Semi-subterranean parking in residential buildings may extend to all property lines, provided that the aboveground section of the semi-subterranean parking garage does not rise more than 4 feet from ground level, does not encroach within or interfere with applicable utility easements, and is completely screened from street view by landscape treatments.

### 4.1.1 Exterior Design

#### *Primary versus Secondary Exposures*

- The primary exposure(s) of buildings are those side(s) of buildings that contain the main entry and face public streets. Primary exposures should receive greater attention to design detail than those that are screened from pedestrian-level, street view by walls, landscaping, or buildings.

Secondary exposures, which are the sides of buildings that face driveways or interior private streets and do not feature the main building entry. While they should be architecturally compatible with the primary exposures, they do not require the same level of detail.

#### *Building Form, Mass, and Scale*

- Encourage sustainable building practices.
- Encourage the use of building techniques and materials that relate to Indio’s warm and dry desert climate. Promote solar control and use of shade in building design and associated pedestrian amenities.

- Simple geometric forms shall constitute the overall building form. Rectangular forms are encouraged to promote balance and visual interest. Arbitrary, complicated building forms shall be avoided.
- Towers and other vertical/prominent building features should be used to accentuate key elements such as building entries and pedestrian plazas or courtyards.
- Multi-family residential buildings shall express a rhythm and pattern through repeated patterns of lines, shapes, forms, and/or colors that reflect the size and scale of a housing unit and/or individual rooms and spaces.
- Buildings greater than three stories in height with building facades greater than 150 feet in length shall have a minimum of one major mass break. Building facades greater than 300 feet in length shall have a minimum of two major mass breaks. Mass breaks shall be a minimum of:
  - Vertical shift modulation of five feet deep and ten feet wide and extend at minimum the full height of the building above the ground floor, including a break in the roofline.
- Building facades greater than 75 feet in length shall have at least one minor mass break. Minor breaks shall be a minimum of:
  - Vertical shift modulation of two feet deep and four feet wide and extend at minimum the full height of the building above the ground floor, including a break in the roofline.
- Reduce the visual impact of long building facades using various paint colors, building materials, recesses, offsets, reveals, decorative fixtures, landscaping, screen walls, and other methods.
- Use building organization and construction to derive scale and articulation rather than surface ornamentation.
- Small-scale articulation of building facades, roofs, materials, colors, and textures are appropriate at locations where pedestrians will experience them.
- Modulation and variation of building masses between adjacent buildings visible from public streets is encouraged.



*Ex: Small-scale articulation, and visually appealing and identifiable entry.*

### Building Articulation

- All facades shall include a minimum of two of the following facade articulation strategies to create visual interest:
  - Change in texture
  - Change in color
  - Change in material
  - Vertical and horizontal recesses such as a pattern of recessed grouping of windows, recessed panels, or similar strategies.
  - Vertical and horizontal projections such as shading and weather protection devices, decorative architectural details, or similar strategies.
  - Datum lines that continue the full length of the building, such as cornices
  - Screening devices such as lattices, louvers, trellis elements, or other similar strategies.
  - Balconies

## Chapter 4 Design Guidelines

- Pilasters
- Material inlays
- Variation of architectural styles
- Building offsets or recesses may be used to accentuate building entries and form courtyards.

### *Building Components (Base/Middle/Top)*

Buildings of three stories or less shall include a defined base and top. Buildings four stories and higher shall be designed to differentiate a defined base, a middle or body, and a top, cornice, or parapet cap. Each of these elements shall be distinguished from one another for a minimum of 80% of the facade length through use of two or more of the following:

- Horizontal facade modulation.
- Horizontal facade articulation and/or variation in facade articulation strategy.
- Variation in facade rhythm and pattern.
- Variation in fenestration strategy (size, proportions, pattern, and depth or projection).
- Variation in material (facade material, material size, texture and/or pattern) and color.

### **Building Base**

- Buildings should rest on a noticeable foundation base or pedestal to visually anchor the structure to the ground unless the Director approves an acceptable alternative.
- When used, the types of materials appropriate for the building base should project an image of durability, quality, and permanence and may include:
  - Plaster
  - Ceramic tile
  - Granite
  - Stone
  - Marble
  - Split-face concrete block



*Ex: Building Component (Base/Middle/Top)*

### **Building Middle**

- Create visual rhythms in the building's middle through repetitive façade elements such as rows of windows and columns.
- Segment the building faces into a series of defined areas using columns, piers, windows, awnings, and other combinations that define the vertical space.

### Building Top

- Buildings should be crowned with a discernible roof cap or edge feature that defines the top of the building unless an acceptable alternative is approved by the Director.
- Appropriate elements for the cap include:
  - Protruding crown molding
  - Roof corbels supporting roof overhangs
  - Roof brackets supporting eave overhangs.
  - Shaped, foam-based, plaster-covered cornice
  - Stepped parapets
  - Textured materials, such as brick masonry
- The types of materials appropriate for the building cap may include:
  - Plaster
  - Ceramic tile
  - Granite
  - Stone
  - Marble
  - Split-face concrete block



*Ex: Primary residential building entry shared by multiple units.*

### Corners and Towers

- When used, tower elements should be located at a focal point, such as the terminus of the main entrance or a major intersection.
- Tower elements or other prominent building features may be used to accentuate key elements such as building entries, converging walls, and pedestrian plazas or courtyards.
- Where appropriate, design building corners at highly visible locations with unique architectural features to emphasize the building entrance.

### Colonnades

- If appropriate, project out or set back upper floors to create colonnades or other pedestrian passages with access to fresh air and natural lighting.
- If used, design colonnades for pedestrian connectivity, protection from the elements, and aesthetic appeal.
- Consistent column placement should define the arcade and frame the building façade.
- Align the openings in the colonnade with doors and windows.

### Building Entry Treatment

- Entries shall be visually appealing and identifiable using elements such as changes in building massing, colors, materials, roof heights, enriched materials, architectural detailing, and the use of tower elements or recesses.

## Chapter 4 Design Guidelines

- Primary residential building entries shared by multiple units (e.g., leading to upper stories) should be accentuated from the overall building façade through a façade modulation that includes at least one of the following:
  - A recess or projection from the primary facade plane with a minimum depth of two feet.
  - A change in roof form.
  - Weather protection that is a minimum of six feet wide and four feet deep by recessing the entry, providing an awning/canopy, or using a combination of these methods.

### Roofs

- Roof styles and materials shall be architecturally and aesthetically compatible.
- Although most roof areas may be flat, the massing of visible elevations shall be broken-up with elements such as sloping roofs, including hips or gable forms (as appropriate to the primary architectural style), variation in roof heights, or parapet treatments with architectural details.
- Roof pitch shall be in proportion to the design of the building and in conformance with code regulations for the roof material.



*Ex: Variation in roof heights and style to reduce massing.*

### Screening of Refuse/Recycling Containers, Utility Equipment, and Mechanical Equipment

- All above-ground utilities and equipment (e.g., electric and gas meters, fire sprinkler valves, irrigation backflow prevention devices, etc.), service areas, and outdoor storage areas shall be integrated into building and landscape design and located to minimize impact on the pedestrian experience and neighboring properties.
- On-site utilities and equipment, service, storage, and non-passenger loading areas shall not be located within 25 feet of a street corner.
- Electrical and gas meters should be located on the rear or side elevation and should be clustered, screened by landscaping or walls, and/or located behind doors. Screening utility meters behind doors is not required where meters are in screened service areas and service courts or in “back of house” areas not intended for public access.
- Screening walls and utility enclosures should be designed as integral architectural features of the building.
- Utilities and equipment, service, refuse and recycling collection or storage areas, and non-passenger loading areas shall be located on non-primary street frontages, alleys, parking areas, and/or at the rear or side of buildings.
- All service and storage areas, utilities, and equipment not housed inside buildings shall be screened so that it is not visible from unobstructed pedestrian-level views from public streets or walkways by utility requirements. Screening may be achieved by the building itself, a screen wall component consistent with the structure’s architecture, landscaping, or other design element that best blends with the overall development.





- Screening shall be made of a primary exterior finish material used on other building sections, architectural grade wood or masonry, metal, or landscape screening that forms an opaque barrier when planted. Roof-mounted mechanical equipment is permitted atop residential structures with flat roofs if screened from view by a parapet designed to be compatible with the building’s architecture, materials, and color.

## 4.1.2 General Architectural Features and Accents

### *Materials and Colors*

- Color and material should be used in a way that is consistent with the overall architectural expression.
- Materials and colors shall match and enhance the architectural style of the building.
- Materials should wrap architectural elements entirely on primary elevations and where exposed to primary public spaces.
- Material changes shall occur at inside corners, edges of building planes, architectural features, building-score lines, or other similar changes in a facade.

### *Windows and Doors*

- Windows, material changes, colors, and façade decorations shall create visual interest and break up flat surfaces, especially at the pedestrian level.
- Window treatments, where feasible, are encouraged. Exterior window treatments include, but are not limited to:
  - Recessing and surrounds of not less than four inches
  - Trim elements
  - Headers and sills
  - Awnings (cloth awnings should be carefully designed due to high winds)
  - Shutters (proportional to window and consistent with architectural style)
  - Mullion patterns as appropriate to the architectural style
- Awnings, when provided, shall be designed to be consistent with the main structure’s architectural style and color palette.

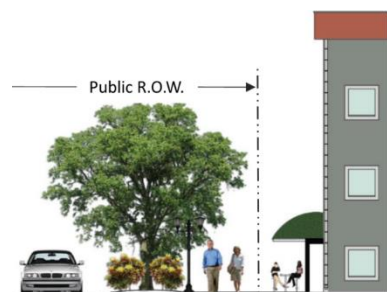
## 4.2 Commercial and Mixed-Use Guidelines

### 4.2.1 Site Design

- Buildings shall be oriented to encourage activity at ground level, provide entrances from the sidewalk, and provide views into other portions of the site.
- Entry courtyards, plazas, entries, or outdoor eating areas should be located to establish connections among buildings and plazas through structural and landscape elements.

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- Provide an array of pedestrian amenities intergraded into the development’s overall design and character of the development, such as seating areas, benches, drinking fountains, landscape



*Ex: Entry and pedestrian activity along public street*

planters, water fountains, shade canopies, and trash receptacles.

- On-site gathering spaces (plazas, courtyards, or similar features) should include pedestrian-scale amenities, such as seating areas, benches, drinking fountains, landscaping planters, water fountains, shade canopies, and trash receptacles.

- Pedestrian amenities such as colonnades, arcades, loggias, arches, and overhangs are encouraged to shade the pedestrian space on either floor of the retail building and at entrances.

- Buildings should be located to define, connect, and activate public and private open spaces as usable plazas, as well as gathering spaces.
- Buildings should be grouped to create a functional and visual relationship between them and clustered when feasible to maximize the use of surface/ground level areas for other uses such as parks, plazas, and paseos.
- Mixed-use projects with residential and commercial uses in the same structure should provide separate entrances for each use.
- For mixed-use, non-residential uses should be located at ground level and oriented to the street. Each space should have individual street-front access to retail/commercial uses. If applicable to the building type, a portion of the ground-level street-oriented space in mixed-use buildings may also be used for residential units.
- Individual street-front access to retail/commercial uses should be provided for each space. A portion of the ground-level street-oriented space in mixed-use buildings may also be used for residential units if applicable to the building type.



*Ex: On-site gathering space and pedestrian amenities.*



*Ex: Outdoor dining area with landscape planters.*

- Outdoor dining areas within front setbacks are encouraged to include planters or other landscape elements.
- Drop-off zones should be located along the curb or within parking facilities to promote sidewalk/street wall continuity and reduce conflicts with pedestrians.

### Parking Areas

- Vehicle parking should be distributed such that all commercial uses are within reasonable walking distance of parking.
- To ensure parking areas do not dominate street frontages, parking lots should be broken up with screen walls, buildings, plazas, or landscaping.

- On-site circulation should be designed to minimize areas of conflict between pedestrians and vehicles.
- On-site pedestrian circulation should include safe pedestrian crossings and direct connections to the retail and service frontage entrances.
- Parking structures may utilize shared entrances for mixed-use developments.



*Ex: Distribution of vehicular parking with safe on-site pedestrian crossings.*

#### Service and Storage Areas

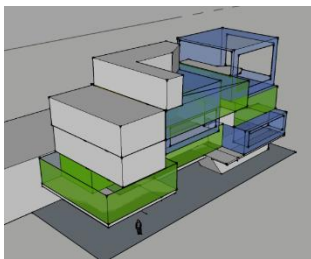
- Service-only entrances should be sited so that they do not interfere with customer access.
- Appropriate on-site service-vehicle parking and turnouts should be provided within efficient, non-obtrusive locations.

### 4.2.2 Exterior Design

#### Primary versus Secondary Exposures

- The primary exposure(s) of buildings are those side(s) of buildings that contain the main entry and face public streets. Primary exposures should receive greater attention to design detail than those that are screened from pedestrian-level street view by walls, landscaping, or buildings.
- Secondary exposures are those side(s) of buildings facing driveways and interior private streets and not containing the main building entry. Secondary exposures shall be architecturally compatible with but need not be as detailed as primary exposures.

#### Building Form, Mass, and Scale



*Ex: Simple geometric forms.*

- Simple geometric forms shall constitute the overall building form. Rectangular forms are encouraged to promote balance and visual interest and arbitrary complicated building forms shall be avoided.
- Towers and other vertical/prominent building features should be used to accentuate key elements such as building entries, as well as pedestrian plazas or courtyards.
- Residential mixed-use buildings should express a rhythm and pattern through repeated patterns of lines, shapes, forms, and/or colors that reflect the size and scale of a housing unit and/or individual rooms and spaces.

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- Buildings greater than three stories in height with facades greater than 150 feet in length shall have at least one major mass break. Building facades over 300 feet in length shall have a minimum of two major mass breaks. Mass breaks shall be a minimum of:
  - Vertical shift modulation of five feet deep and ten feet wide and extend at minimum the full height of the building above the ground floor, including a break in the roofline.
- Building facades greater than 75 feet in length shall have at least one minor mass break. Minor breaks shall be a minimum of:
  - Vertical shift modulation of two feet deep and four feet wide and extend at minimum the full height of the building above the ground floor, including a break in the roofline.
- Reduce the visual impact of long building facades using various paint colors, building materials, recesses, offsets, reveals, decorative fixtures, landscaping, screen walls, and other methods.
- Use building organization and construction to derive scale and articulation rather than surface ornamentation.
- Small-scale articulation of building facades, roofs, materials, colors, and textures can significantly enhance the pedestrian experience, particularly in areas where they are most likely to be noticed.
- Modulation and variation of building masses between adjacent buildings visible from public streets are encouraged.



*Ex: Building form, mass, and scale.*

### Building Articulation

- All facades shall include a minimum of two of the following facade articulation strategies to create visual interest:
  - Change in texture
  - Change in color
  - Change in material
  - Vertical and horizontal recesses such as a pattern of recessed grouping of windows, recessed panels, or similar strategies.
  - Vertical and horizontal projections such as shading and weather protection devices, decorative architectural details, or similar strategies.
  - Datum lines that continue the full length of the building, such as cornices.
  - Screening devices such as lattices, louvers, trellis elements, or other similar strategies.
  - Balconies
  - Pilasters
  - Material inlays



*Ex: Building articulation through use of vertical and horizontal projections, trellis elements, and changes in color, texture, and material.*

- Variation of architectural styles
- Building offsets or recesses may be used to accentuate building entries and form courtyards.
- Facade articulation is expected to be used for commercial facing I-10, but at a scale appropriate to fast-moving vehicles and not for pedestrians.

*Building Components (Base/Middle/Top)*

Buildings of three stories or less shall include, at minimum, a defined base and top. Buildings four stories and higher shall be designed to differentiate a defined base, a middle or body, and a top, cornice, or parapet cap. Each of these elements shall be distinguished from one another for a minimum of 80% of the facade length through use of two or more of the following:

- Horizontal facade modulation.
- Horizontal facade articulation and/or variation in facade articulation strategy.
- Variation in facade rhythm and pattern.
- Variation in fenestration strategy (size, proportions, pattern, and depth or projection).
- Variation in material (facade material, material size, texture, and/or pattern) and color.

**Building Base**

- Buildings should rest on a noticeable foundation base or pedestal to visually anchor the structure to the ground unless the Director approves an acceptable alternative.
- When used, the materials appropriate for the building base should project an image of durability, quality, and permanence.



*Ex: Building Component (Base/Middle/Top)*

**Building Middle**

- Create visual rhythms in the building’s middle through repetitive façade elements such as rows of windows and columns.
- Segment the building faces into a series of defined areas using columns, piers, windows, awnings, and other combinations that define the vertical space.

**Building Top**

- Buildings should be crowned with a discernible roof cap or edge feature that defines the top of the building unless an acceptable alternative is approved by the Director.
- Appropriate elements for the cap include:
  - Protruding crown molding
  - Roof corbels or brackets supporting roof overhangs
  - Shaped, foam-based, plaster-covered cornice
  - Stepped parapets
  - Textured materials, such as brick masonry

## Chapter 4 Design Guidelines

### Corners and Towers

- When used, tower elements should be located at a focal point, such as the terminus of the main entrance or a major intersection.
- Tower elements or other prominent building features shall be used to accentuate key elements such as building entries, converging walls, and pedestrian plazas or courtyards.
- Where appropriate, design building corners at highly visible locations with unique architectural features to emphasize the building entrance.



### Colonnades

- If appropriate, project out or set back upper floors to create colonnades or other pedestrian passages with access to fresh air and natural lighting.
- If used, design colonnades for pedestrian connectivity, protection from the elements, and aesthetic appeal.
- Consistent column placement should define the arcade and frame the building façade.
- Align the openings in the colonnade with doors and windows.

### Building Entry Treatment

- Entries shall be visually appealing and identifiable using elements such as changes in building massing, colors, materials, roof heights, enriched materials, architectural detailing, and the use of tower elements or recesses.
- Primary entries should be designed so they are easily recognized, are the most prominent entry for that tenant, and are readily accessible to pedestrians and near parking.
- Secondary entries should also be easily recognizable but designed at a lesser scale than the primary entry.

### Roofs

- Roof styles and materials shall be architecturally and aesthetically compatible.
- Although many roof areas may be flat, the massing of visible elevations should be broken-up with elements such as sloping roofs, including hips or gable forms (as appropriate to the primary architectural style), variation in roof heights, or parapet treatments with architectural details.
- Roof pitch shall be in proportion to the design of the building and in conformance with code regulations for the roof material.

### *Screening of Refuse/Recycling Containers, Utility Equipment, and Mechanical Equipment*

- All above-ground utilities and equipment (e.g., electric and gas meters, fire sprinkler valves, irrigation backflow prevention devices, etc.), service areas, and outdoor storage areas should be

integrated into building and landscape design and located to minimize impact on the pedestrian experience and neighboring properties.



- On-site utilities and equipment, service, storage, and non-passenger loading areas shall not be located within 25 feet of a street corner.
- Electrical and gas meters should be located on the rear or side elevation and should be clustered, screened by landscaping or walls, and/or located behind doors. Screening utility meters behind doors is not required where meters are in screened service areas and service courts or in “back of house” areas not intended for public access.

- Screening walls and utility enclosures should be designed as integral architectural features of the building.
- Utilities and equipment, service, refuse and recycling collection or storage areas, and non-passenger loading areas should be located on non-primary street frontages, alleys, parking areas, and/or at the rear or side of buildings.
- Refuse collection areas to the extent possible shall be located as far as possible from the residential portion of mixed-use buildings and open space areas.
- All service and storage areas, utilities, and equipment not housed inside buildings shall be screened so that it is not visible from unobstructed pedestrian-level views from public streets or walkways in accordance with utility requirements. Screening may be achieved by the building itself, a screen wall designed as an integral architectural feature of the building, landscaping, or other design element that best blends with the overall development.
- Screening shall be made of a primary exterior finish material used on other portions of the building, architectural grade wood or masonry, metal, or landscape screening that forms an opaque barrier when planted.
- Roof-mounted equipment shall be screened so that it is not visible from unobstructed pedestrian-level views from public streets, walks, or parking lots. Screening of rooftop equipment may be achieved by the building parapet or other similar screen wall component that is consistent with the structure’s architecture. This applies to all types of equipment, including mechanical equipment, communication dishes, skylights, exhaust fans, ducts, or any other non-architectural elements.



### 4.2.3 General Architectural Features and Accents

- One architectural style shall apply to all buildings within the commercial/retail land use planning area, although variation from the chosen theme is allowed for major national retail chains that must maintain certain characteristics for identification.
- Variation on the architectural theme is encouraged through simple changes such as color palette and entry treatments.

## Chapter 4 Design Guidelines

### *Materials and Colors*

- Color and material should be used in a way that is consistent with the overall architectural expression.
- A dominant building material and color should be clearly established in each building. Accents and variations may then occur within the background established by that dominant base. The dominant colors for buildings should be more neutral, with the more intense colors used in the accents.
- Materials and colors should match and enhance the architectural style of the building. The selection of materials and colors should not be arbitrary, but rather a thoughtful process that matches and enhances the architectural style of the building. This approach ensures a harmonious and aesthetically pleasing design.
- All materials shall wrap architectural elements entirely on primary elevations and where exposed to primary public spaces.
- Material changes shall occur at inside corners, edges of building planes, changes in architectural features, building-score lines, or other similar changes in a façade.

### *Windows and Doors*

- Windows, material changes, colors, and façade decorations shall be used to create visual interest and break up flat surfaces, especially at the pedestrian level.
- Window treatments, where feasible, are encouraged. Exterior window treatments include, but are not limited to:
  - Recessing and surrounds of not less than four inches
  - Trim elements
  - Headers and sills
  - Awnings (cloth awnings are typically not durable against the Santa Ana high winds and therefore, not preferred. If used, they shall be carefully designed to withstand these conditions)
  - Shutters (proportional to the window and consistent with architectural style)
  - Mullion patterns as appropriate to the architectural style
- Awnings, when provided, shall be designed to be consistent with the main structure's architectural style and color palette.
- Avoid continuous awnings. Awnings should frame a segmented portion of the storefront, such as a window bay or entrance.
- Emergency exit or egress-only doors should be treated to blend in with the adjacent walls or surfaces to discourage their perception as entries.

## 4.3 Industrial Guidelines

### 4.3.1 Site Design

- Buildings should be oriented to facilitate the ease of truck parking and loading.
- Buildings should be designed with the vehicle in mind, requiring less pedestrian-scale articulation than buildings in the other land use planning areas.

- Interior driveways and drive aisles shall be designed to provide adequate stacking and prevent vehicles queuing on public streets.
- Service entrances shall be sited to not interfere with owner/tenant/customer access.
- Loading areas shall be designed for backing and maneuvering on-site, not from a public street.
- Appropriate on-site service vehicle parking/turnouts shall be provided in an efficient, nonobtrusive location appropriate to the development’s scale and needs.
- Loading vehicles, when parked, shall not impede normal traffic flow.

### 4.3.2 Exterior Design

#### Primary versus Secondary Exposures

- The primary exposure(s) of buildings are those side(s) that contain the main entry and face “A” Street. Primary exposures should receive greater attention to design detail than those that are screened from pedestrian-level, street view by walls, landscaping, or buildings. This can be achieved through form, mass, scale, modulation, and architectural detail variation.
- Secondary exposures are those side(s) of buildings that face driveways and do not contain the main building entry.
- Secondary exposures shall be architecturally compatible with but need not be as detailed as primary exposures.



*Ex: Secondary exposure side of a building architecturally compatible to the primary exposure of the building through use of windows and color blocking.*

#### Building Form, Mass, and Scale

- Simple geometric forms shall constitute the overall building form. Rectangular forms are encouraged to promote balance and visual interest. Arbitrary complicated building forms shall be avoided.
- Reduce the visual impact of long building facades using various paint colors, building materials, recesses, reveals, offsets, decorative fixtures, landscaping, screen walls, and other methods.
- Small-scale articulation of building facades, roofs, materials, colors, and textures are appropriate at locations where pedestrians will experience them.
- Long horizontal wall planes visible from a public street shall include periodic changes in exterior building materials, color, decorative accents, and/or articulated features.
- Modulation and variation of building masses between adjacent buildings visible from public streets is encouraged.



*Ex: High level of building articulation, architectural elements, and textured materials reduce the visual impact of a long building façade.*

## Chapter 4 Design Guidelines

- Pedestrian entrances to buildings should be made obvious through changes in massing, color, and/or building materials.
- Pedestrian and ground-level building entries should be recessed or covered by architectural projections, roofs, or arcades to provide shade and visual relief.
- Architectural and trim detailing on building façades shall be clean, simplistic, and not overly complicated.
- Materials applied to any elevations shall turn the corner of the building to a logical termination point in relation to architectural features or massing.

### Building Articulation

#### Building Body

- Create visual rhythms in the building's middle through repetitive façade elements such as rows of windows and columns.
- Segment the building faces into a series of defined areas using columns, scoring, piers, and other combinations that define the vertical space.



*Ex: Visual rhythm using rows of windows.*

#### Corners and Towers

- When used, towers and other vertical/prominent building features should be used to accentuate key elements such as building entries, pedestrian plazas or courtyards, terminus of the main entrance, converging walls, or at a major intersection.
- Design building corners at highly visible locations with a unique architectural feature to emphasize the building entrance.



*Ex: Detailed façade design highlights the covered building entrance and creates a recognizable entrance.*

#### Building Entry Treatment

- Entries shall be visually appealing and identifiable using elements such as changes in building massing, colors, materials, roof heights, enriched materials, architectural detailing, and the use of tower elements or recesses.
- Primary entries should be designed so they are easily recognized, the most prominent entry for that tenant, readily accessible to pedestrians, and near parking.
- Secondary entries should also be easily recognizable but designed at a lesser scale than the primary entry.
- When building entries are designed with a discernible roof cap or edge, appropriate elements include:
  - Protruding crown molding

- Roof corbels supporting roof overhangs
- Roof brackets supporting ease overhangs.
- Shaped, foam-based, plaster-covered cornice
- Stepped parapets
- Textured materials, such as brick masonry

### *Roofs*

- Roof styles and materials shall be architecturally and aesthetically compatible.
- Although most roof areas may be flat, the massing of visible elevations should be broken-up with elements such as variation in roof heights or parapet treatments with architectural details.
- Arched, gable, hip, and shed roof forms shall not be used as a primary roof form but as a secondary/accent roof form.
- All rooftop-mounted equipment shall be screened from public view using materials complementary to those used on the main structure.

### *Screening of Refuse/Recycling Containers, Utility Equipment, and Mechanical Equipment*

- It is crucial to integrate all above-ground utilities and equipment (e.g., electric and gas meters, fire sprinkler valves, irrigation backflow prevention devices, etc.), service areas, and outdoor storage areas into building and landscape design. This integration should be done with the aim to minimize impact on the pedestrian experience and neighboring properties.
- Electrical and gas meters shall be located on the rear or side elevation and clustered, screened by landscaping or walls, and/or located behind doors. Screening utility meters behind doors is not required where meters are in screened service areas and service courts or in “back of house” areas not intended for public access.
- The design of screening walls and utility enclosures should be approached as integral architectural features of the building. This approach ensures that they not only serve their functional purpose but also contribute to the overall aesthetic of the structure.
- Whenever possible, refuse and recycling collection or storage areas should be located behind or to the side of the building served.
- Solid walls, landscaping, or trellis work shall screen refuse containers, utility equipment, and mechanical equipment from unobstructed pedestrian-level, street-view streets. This screening shall be conceived as part of the building architecture and landscape design to best blend with the overall development.

## Chapter 4 Design Guidelines

- Roof-mounted equipment shall be screened so that it is not visible from unobstructed pedestrian-level views from public streets, walks, or parking lots. Screening of rooftop equipment may be achieved by the building parapet or other similar screen wall components that are consistent with the structure's architecture. This applies to all types of equipment, including mechanical equipment, communication dishes, skylights, exhaust fans, ducts, or any other non-architectural elements.
- All wall- or ground-mounted equipment shall be screened so that it is not visible from unobstructed pedestrian-level views from public streets or walkways in accordance with utility requirements. Screening equipment may be achieved by the building itself, a screen wall component that is consistent with the structure's architecture, landscaping, and other devices acceptable to the City.



*Ex: Quality material screening of loading docks.*

### Screening of Loading Areas

- Loading docks and doors facing I-10 are not required to be entirely blocked from view but will include landscape treatments.
- Truck loading docks shall be screened from pedestrian-level, public view from the streets as detailed in Chapter 3, *Development Standards*. Screening may be aesthetically compatible landscaping or comparable materials that blend with the architectural and landscape treatments of the site and perimeter landscaping.

### 4.3.3 General Architectural Features and Accents

- Architectural design should be clean, simple, and streamlined for a modern appearance.
- Variation on the architectural theme is encouraged through simple changes such as color palette and entry treatments.

*Materials and Colors*

- All facilities shall be constructed of permanent finished materials such as concrete, masonry, and glass. Acceptable exterior finish materials include concrete with textured, sandblasted, or painted finishes; stone or brick; stucco; or masonry with textured or sandblasted finishes. Glass, glazing systems, glass block, ceramic or natural stone tile, decorative metal, and metal panel systems are appropriate for accents. Metal siding may be used as an architectural detail or only when it serves a practical purpose (e.g., refrigeration units) and is limited to 15 percent of an elevation.
- Exterior building colors are light and warm tones with darker or more vibrant accent colors on wall planes at focal points, such as around building entrances and near outdoor gathering spaces. Additionally, architectural designs may mix colors, materials, and textures to articulate façades and create visual appeal. Materials and colors shall match and enhance the architectural style of the building.
- Material changes shall occur at corners, edges of building planes, architectural features, building-score lines, or other similar changes in a facade.
- Materials applied to any elevations shall turn the corner of the building to a logical point in relation to architectural features or massing.



*Ex: Light and warm hues on the building facade with more vibrant accent colors to emphasize building entry.*

*Windows and Doors*

- Windows, doors, and other openings should unify the building façade by creating a clear pattern.
- Window treatments, where feasible, are encouraged. Exterior window treatments include, but are not limited to:
  - Recessing and surrounds of not less than four inches
  - Trim elements
  - Headers and sills
  - Mullion patterns as appropriate to the architectural style
- Awnings, when provided, shall be designed to be consistent with the architectural style and color palette of the main structure.
- Windows, material changes, colors, and façade decorations shall create visual interest and break up flat surfaces, especially at the pedestrian level.
- Emergency exit or egress-only doors should be treated to blend in with the adjacent walls or surfaces to discourage their perception as entries.



*Ex: Window design creates a horizontal emphasis and frames the primary building entry. Awnings are consistent with the architectural style.*

## 4.4 Walls and Fences

Walls and Fences will support the goals of the master plan with visually cohesive designs that reflect the contemporary village aesthetic yet are unifying within the overall community. In mixed-use areas and industrial areas facing local roads, materials will complement the adjacent architecture while maintaining an understated contemporary quality: precision block walls with running or stacked patterns, stucco finishes in light colors, tubular steel fences with minimal detailing and dark bronze or matte black finishes.

Industrial areas facing freeways, arterials and open space may also include tubular steel fences with extended pickets (for security), heavy gauge wire mesh and vinyl-coated chain link in color ranges described above.

## 4.5 Landscape



The landscape at The Oasis of Indio will support the goals of the Specific Plan through visually cohesive landscaped spaces within the contemporary village. Landscape areas will provide a sense of arrival, offer clear and safe pathways for interconnectivity, and invite visitors to stay while providing visual continuity.

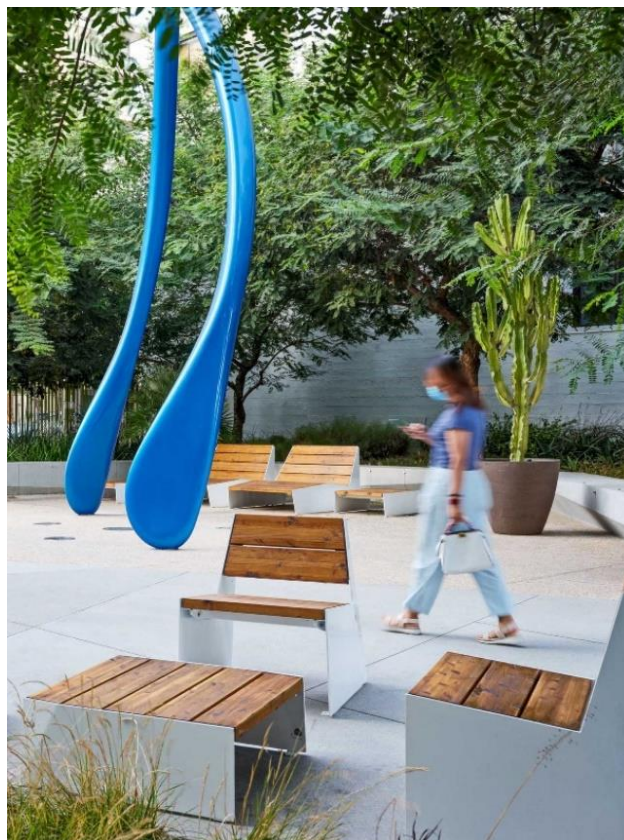
An emphasis will be placed on pedestrian-oriented experiences that incorporate a mix of materials, complementing the adjacent architecture.



- Shade canopies and trellises shall be used to add architectural interest to the landscape while providing visual interest and individuality to each land use district. Seasonal shading concepts shall be considered for plazas and seating areas to address the variation in seasonal temperatures.
- Large specimen shade trees should be considered within plaza spaces to create inviting destinations and a sense of permanence. A thoughtful selection of trees, plant species, and comfortable site furnishings will contribute to the enjoyment of these spaces.
- Conveniently placed bike racks near commercial corridors and visible from a variety of destinations will increase the use of modes of transportation.  
Select gathering areas shall be visible from Monroe Street and "A" Street to energize the street presence and invite visitors.

## 4.6 Landscape Architectural Concepts

The landscape concept within The Oasis at Indio will reinforce the contemporary mixed-use village while providing visual cues for visitors and inviting destinations. Landscape features that complement the contemporary aesthetic while also providing functionality and sustainability shall be incorporated into the design of the residential, commercial/retail, and industrial land use areas. Each area shall have a distinct “personality” that supports the adjacent uses while lending individuality to each land use



district.

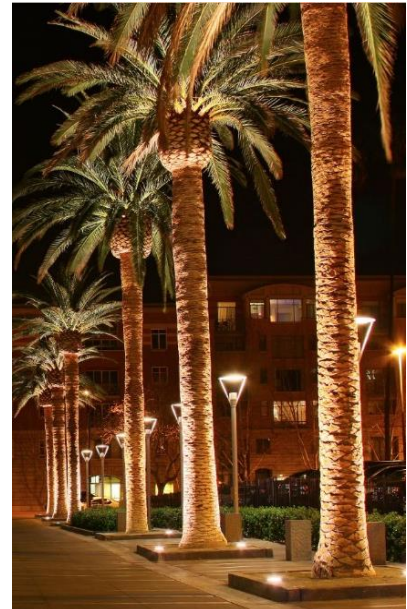
The landscape design shall utilize a hierarchy that provides visually prominent planting for the purpose of inviting and guiding, and a more visually receding landscape for more functional spaces.

Some guiding ideas:

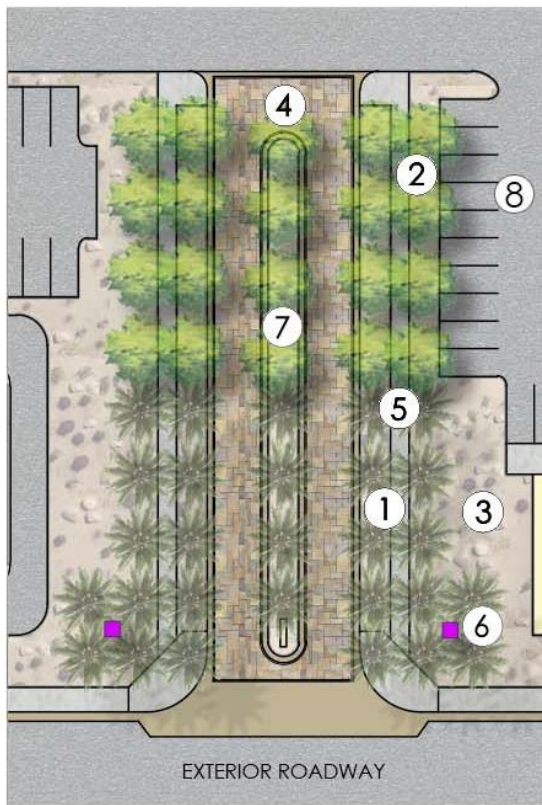
- Resort-style palm trees at commercial/retail and multi-family entries
- Desert shade trees at industrial entries and parking areas
- Formal use of planting at entries, gateways, and transition areas
- Large evergreen shade trees and palms at plazas and seating areas

## 4.7 Entries, Edge Conditions and Social Spaces

The following design studies are intended to guide the hierarchy described above.



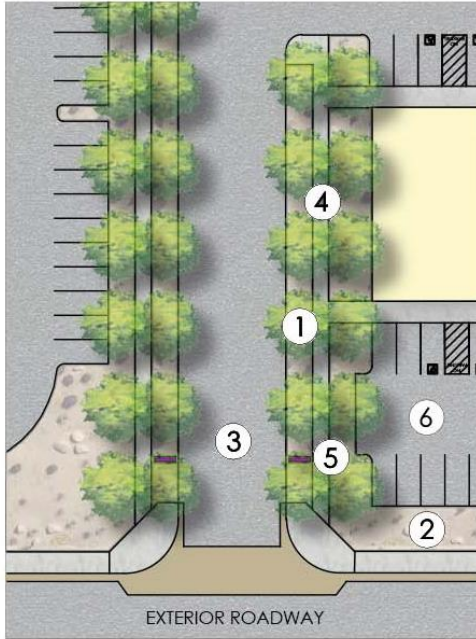
### Retail and Multi-Family Entry



#### Legend

1. Date Palm tree
2. Desert canopy tree
3. Desert landscape areas
4. Enhanced pavement
5. 6' wide concrete walkway
6. Horizontal or vertical marker
7. Median Island
8. Parking lot/Driveway

## Industrial Entry



### Legend

1. Desert canopy tree
2. Desert landscape areas
3. Asphalt Pavement
4. 6' wide concrete walkway
5. Horizontal or vertical marker
6. Drive Aisle

## Corner Monumentation

The landscape corner monuments located at primary intersections shall follow the current sign hierarchy program located on Monroe Street north of the 1-10 freeway. The sign size, orientation, and associated landscape areas will reflect the scale of the adjacent retail property. Plant material shall include mature Date Palm trees and create an attractive landscape composition with the sign and wall design.



## Signature District Style



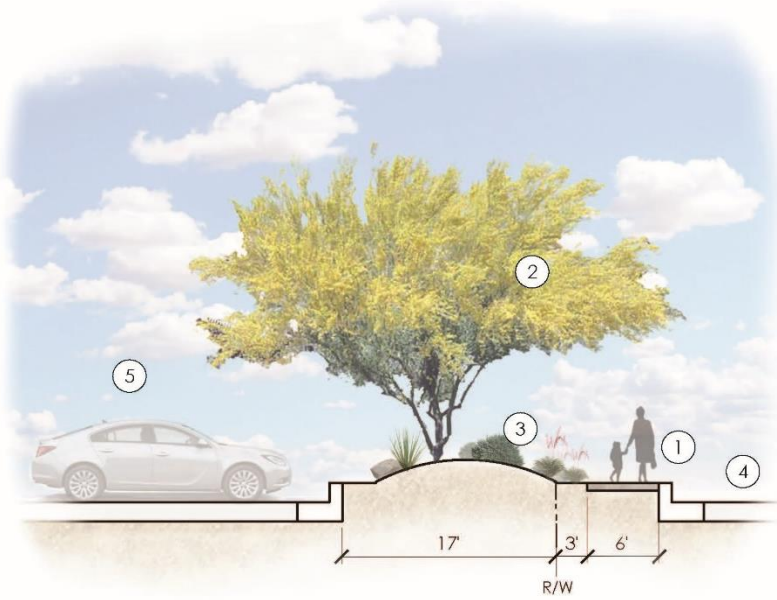
Each planning area is encouraged to create a signature style through a mix of planting and hardscape materials. Hardscape materials may include stone, stucco, masonry, precast concrete, and wood. Planting areas can be architectural with symmetrical rows or eclectic designs with varied textures and forms.

Screening and buffering adjacent conditions will be required for successfully integrating into the surrounding neighborhood (exhibits shown below on page 4-26 through 4-29). Some edge requirements include:

- Monroe Street Parkway (Figure 4-1): 20' landscape area with a 2-3' high berm to help screen vehicles in adjacent parking lots from the roadway.
- A Street @ Commercial Edge Condition (Figure 4-2): 21' landscape area with 2-3' high berm to help screen vehicles in adjacent parking lots from the roadway.
- A Street @ Industrial Edge (Figure 4-3): 14' landscape area with 2-3' high berm and desert canopy tree adjacent to 12' screen wall.
- Freeway Edge (Figure 4-4): 10' landscape area with vertical/upright trees to help screen industrial building(s).
- Avenue 42 @ Residential Edge (Figure 4-5): A 19' landscape area with a high wall or fence and desert canopy trees or palms within the walled area.
- A Street @ Residential Edge (Figure 4-6): 19' landscape area with decomposed granite trail connecting to adjacent 35 to 50-foot-wide landscaped detention basin zone (may include access drives and/or walks from northerly mixed-use areas).
- A Street Cross Section (Figure 4-7): provided for illustrative purposes.

Providing shade for both pedestrians and vehicles is required in the local desert climate. Parking areas shall focus on effective shade through a consistent use of canopy trees that thrive in the desert environment. Shaded walkways will promote comfortable connections to each district. Utilizing a variety of evergreen and deciduous trees at seating areas will provide seasonally adapted shade.

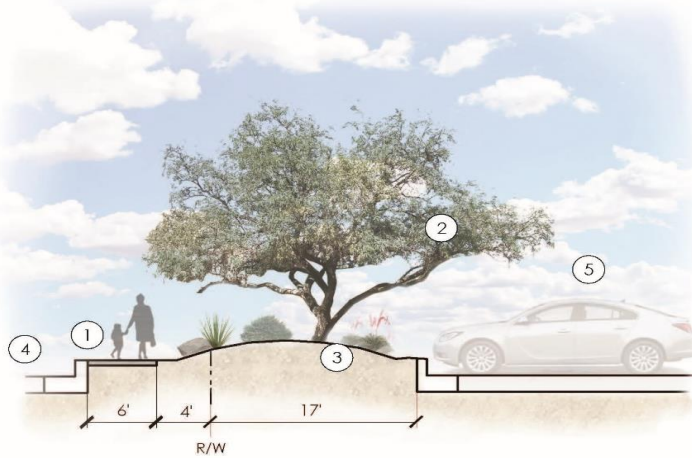
Chapter 4 Design Guidelines



Legend

- 1. 6' Sidewalk
- 2. Desert canopy tree
- 3. 2'-3' berm with water efficient landscape
- 4. Monroe Street
- 5. Commercial parking lot

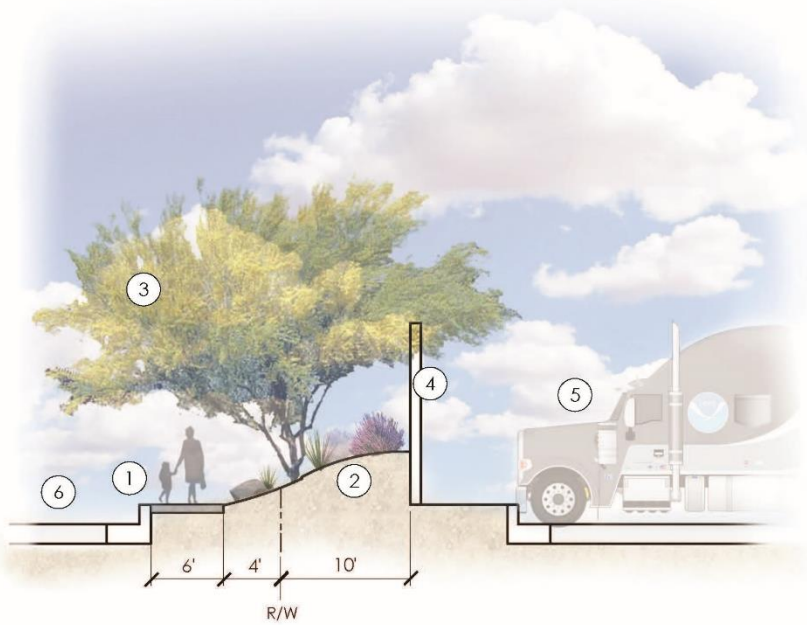
Figure 4-1: Monroe Street Parkway – Commercial Edge Condition (West)



Legend

- 1. 6' Sidewalk
- 2. Desert canopy tree
- 4. 2'-3' berm with water efficient landscape
- 4. Street 'A'
- 5. Commercial parking lot

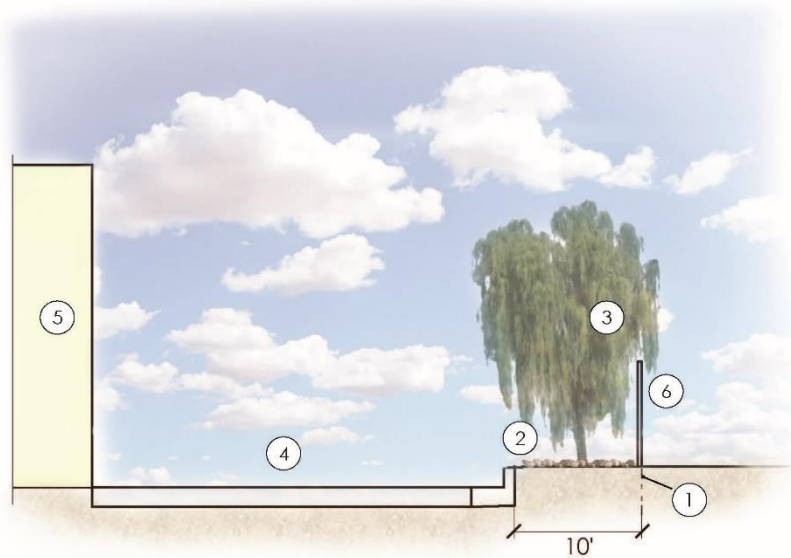
Figure 4-2: Street 'A' – Commercial Edge Condition



Legend

- 1. 6' Sidewalk
- 2. 2'-3' berm with water efficient landscape
- 3. Desert canopy tree
- 4. 12' Screen wall
- 5. Truck parking
- 6. Street 'A'

Figure 4-3: Street 'A' – Industrial Edge Condition



Legend

- 1. Property Line
- 2. Crushed Rock Groundcover
- 3. Vertical/Upright Trees
- 4. Truck Loading Dock
- 5. Industrial Building
- 6. 8' Ht. Fence

Figure 4-4: Freeway Edge Condition

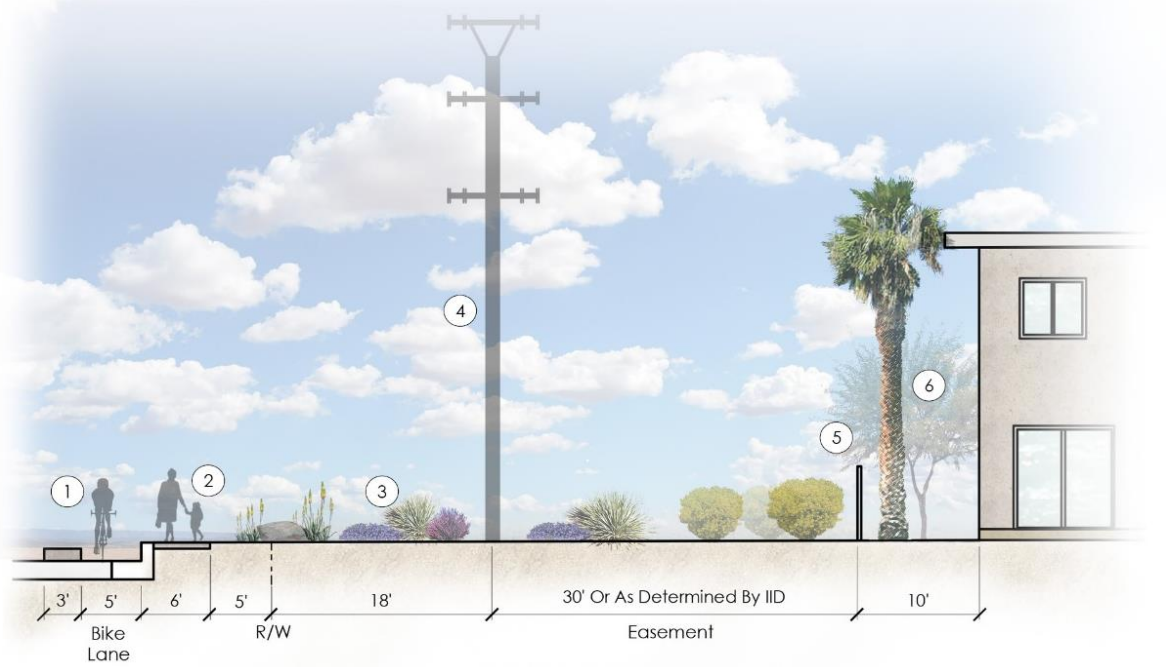
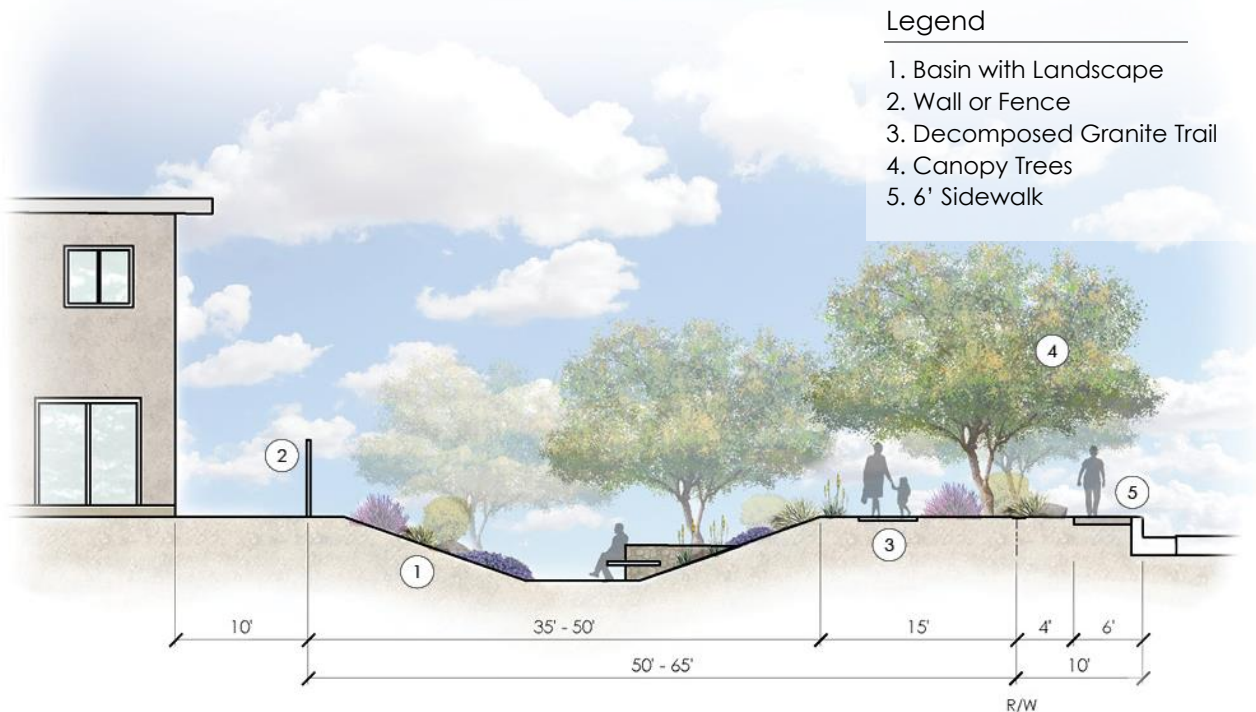


Figure 4-5: Avenue 42 – Residential Edge Condition

Legend

- |                              |                     |
|------------------------------|---------------------|
| 1. 5' Bike Lane              | 4. Power Pole       |
| 2. 6' Sidewalk               | 5. 6' Wall or Fence |
| 3. Water Efficient Landscape | 6. Desert Trees     |



Legend

- |                             |
|-----------------------------|
| 1. Basin with Landscape     |
| 2. Wall or Fence            |
| 3. Decomposed Granite Trail |
| 4. Canopy Trees             |
| 5. 6' Sidewalk              |

Figure 4-6: Street 'A' Residential Edge Condition

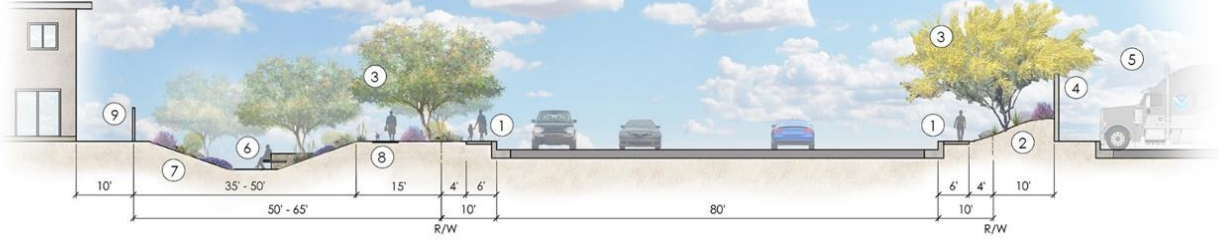


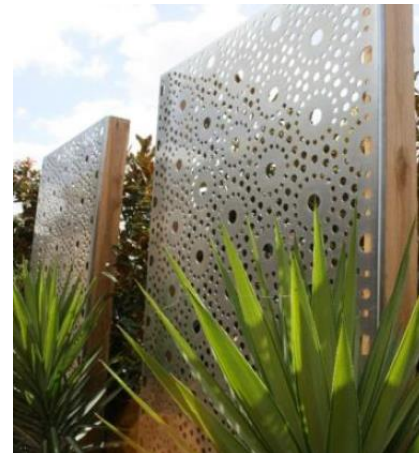
Figure 4-7: Street "A" Cross Section

Legend

- |                                    |   |
|------------------------------------|---|
| 1. 6' Sidewalk                     | 6. Gabion Wall with Seating             |
| 2. 2'-3' Berm with Desert Planting | 7. Basin with Water Efficient Landscape |
| 3. Desert Canopy Trees             | 8. Decomposed Granite Walking Trail     |
| 4. 12' Screening Wall              | 9. Wall or Fence                        |
| 5. Truck Parking                   |   |

Drive-Through Screening

Creative solutions for screening undesirable views shall include berms, multiple layers of landscaping and architectural screens that complement the building architecture. Architectural planting solutions and modern interpretive wall art may be used for creating interest along perimeter walls.



## Chapter 4 Design Guidelines

### *Detention Basins*

Detention basins shall include desert riparian trees, desert cactus and succulents, and select



groundcovers combined with rock outcroppings. Gathering areas with gabiion structures are encouraged around the basins to activate these open spaces. Natural, undulating landforms are preferred over geometrically graded basins.

## 4.8 Landscape Sustainability and Energy Efficiency

The plant palette throughout the Specific Pan area shall utilize a low-maintenance and low-water palette. Turf grasses will only be permitted in “use” areas that include open lawns for play/ recreation and infiltration portions of detention basins. Annual plant materials are allowed in accent landscaping areas but shall not be allowed in permanent planting areas of the Specific Plan.

The landscaping and irrigation plans and system shall comply with all City ordinances relating to water efficiency and shall be an automatic system with an irrigation timer and two drip or bubbler heads per tree to produce deep-root irrigation. The water use calculations for this system will be prepared and submitted per Indio Water Authority’s irrigation design requirements.

Landscape lighting will include tree and shrub-up lights, path lights, and step lights. All lighting will be low voltage and have low-maintenance LED fixtures. The selective use of uplights will be limited to high-profile specimen trees and shrubs and will be shielded to reduce glare and hot spots. There shall be a lumen foot candle for all exterior lighting at any property line of abutting properties and/or public right-of-way. Lighting for landscaping, pathways, stairways, monuments, and signs shall be installed as needed for security and safety purposes.

## 4.9 'THE OASIS OF INDIO' PLANT PALETTE

<b>Trees</b>	<b>Botanical / Common Name</b>
	Acacia aneura / Mulga Tree
	Acacia salicina / Willow Acacia
	Caesalpinia cacalaco 'Smoothie' / Cascalote
	Cercidium X 'Desert Museum' / Desert Museum Palo Verde
	Chilopsis linearis / Desert Willow
	Olea europaea 'Swan Hill' / Fruitless Olive Tree
	Olneya tesota / Desert Ironwood
	Parkinsonia praecox / Sonoran Palo Verde
	Prosopis glandulosa 'Maverick' / Thornless Honey Mesquite
	Tipuana tipu/ Tipu Tree
<b>Palm Trees</b>	<b>Botanical / Common Name</b>
	Phoenix roebelenii / Pygmy Date Palm
	Phoenix dactylifera / Date Palm
	Washingtonia robusta / Mexican Fan Palm
	Washingtonia filifera / California Fan Palm
<b>Shrubs</b>	<b>Botanical / Common Name</b>
	Abronia vilosa / Desert Sand Verbena
	Bougainvillea x 'Oo La-La'
	Caesalpinia gilliesii / Yellow Bird of Paradise
	Cascabela thevetia / Yellow Oleander
	Callistemon viminalis 'Little John' / Dwarf Bottlebrush
	Chrysactinia mexicana / Damianita
	Dasyliion wheeleri / Desert Spoon
	Dianella tasmanica / Tasman Flax Lily
	Eremophila glabara / Mingenew Gold
	Eremophila hygrophana / Blue Bells
	Euphorbia milii / Crown of Thorns
	Glandularia pulchella / Moss Verbena
	Hesperaloe spp.
	Ikora coccinea / Maui Yellow
	Lantana c. 'Dallas Red' / Red Lantana
	Lantana x 'New Gold'
	Leucophyllum frutescens 'Compacta' / Compact Texas Ranger

	Leucophyllum langmaniae 'Rio Bravo' / Texas Ranger
	Leucophyllum laevigatum / Chihuahuan Sage
	Muhlenbergia capillaris 'Regal Mist' / Pink Muhly Grass
	Muhlenbergia lindheimeri / Lindheimer's Muhly
	Olea e. 'Montra' / Little Ollie Dwarf Olive
	Russelia equisetiformis / Coral Fountain
<b>Accents</b>	<b>Botanical / Common Name</b>
	Agave americana / Century Plant
	Agave desmettiana / Smooth Agave
	Agave geminiflora / Twin Flowering Agave
	Agave ocahui / Century Plant Agave victoriae-reginae / Queen Victoria Agave
	Agave weberi / Weber Agave
	Agave x 'Blue Glow' / Blue Glow Agave
	Echinocactus grusonii / Golden Barrel Cactus
	Euphorbia resinifera / Resin Spurge
	Yucca aloifolia / Spanish Bayonet
	Yucca pendula / Soft Leaf Yucca
	Yucca rostrata / Beaked Yucca
<b>Vine / Espalier</b>	<b>Botanical / Common Name</b>
	Bougainvillea X 'Barbara Karst'
	Calliandra haematocephala / Red Powder Puff
<b>Crushed Stone</b>	<b>Common Name</b>
	* Baja Cresta Rubble
	* Copper Canyon
	* Mojave Gold
	* Palomino Coral
<b>DG (Decomposed Granite)</b>	<b>Common Name</b>
	* Desert Gold
	* Mojave Gold
	* Palomino Coral
* Note: Available through Southwest Boulder Palm Springs, or approved equal	

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# Chapter 5– Administration and Implementation

The purpose of this section is to provide an outline of the steps necessary to implement The Oasis at Indio Specific Plan for the benefit of the development team, the City and other approving agencies, and interested citizens.

## 5.1 Administration

The California Government Code (Title 7, Division 1, Chapter 3, Article 8, Sections 65450 et seq.) grants authority to cities to use Specific Plans for purposes of implementing the goals and policies of the City’s General Plan.

The Oasis at Indio Specific Plan establishes a set of regulations, standards, guidelines, and processes for the proposed development, and shall constitute the zoning for development within the Specific Plan Area.

### 5.1.1 Responsibility and Enforcement

The City of Indio Planning and Building and Safety Division, Community Development Director, or his or her designee shall be responsible for administering The Oasis at Indio Specific Plan in accordance with the provisions of this Specific Plan document, all governing and applicable State and federal laws, the City of Indio’s General Plan, and the City of Indio’s Municipal Code.

The Oasis at Indio Specific Plan serves as the implementation tool for the General Plan land use and zoning for the Specific Plan Area. The Specific Plan addresses permitted uses, development standards, and design guidelines. The City shall enforce the provisions of the Specific Plan in the same manner that the City enforces the provisions of the Municipal Code and Unified Development Code.

### 5.1.2 Applicability

All development within The Oasis at Indio Specific Plan Area shall comply with the requirements and standards set forth in this Specific Plan. If conflicts exist between the standards contained in this Specific Plan, Unified Development Code, or Municipal Code, the regulations and standards in the Specific Plan shall take precedence.

Any area of site development, administration, review procedures, and regulations not expressly addressed by this Specific Plan document shall be subject to the provisions of the City of Indio Unified Development Code (IUDC) or Municipal Code, using the context and objectives of the Specific Plan as a guide.

### 5.1.3 Severability

If any portion of this Specific Plan and its regulations are declared to be invalid or in effect in whole or in part by a court of competent jurisdiction, such decision shall not affect the validity of the remaining portions thereof.

### 5.1.4 Interpretation

Whenever the provisions contained in the Specific Plan conflict with the Municipal or Unified Development Code, the provisions of the Specific Plan shall take precedence. In case of uncertainty or ambiguity to the meaning or intent of any provision concerning the content or application of The Oasis at Indio Specific Plan, the Community Development Director or designee has the authority to interpret the intent of the provision. Such interpretations shall take into account the stated goals and objectives of the Specific Plan. The Community Development Director's decision shall be final.

### 5.1.5 Initial Entitlements

Initial entitlements required for development in the Specific Plan Area include the following actions to be taken by the City:

- Environmental Impact Report (EIR)
- General Plan Amendment – The 2040 General Plan is a long-term comprehensive policy document that contains the primary vision, principles, and policies that define the future land use mixes, intensities, patterns, and forms to be implemented. The General Plan Amendment will be considered by the Planning Commission and City Council and adopted by Resolution. The General Plan Amendment proposes to amend the Land Use Element, Figure 3-4 “General Plan Place Types” from Mixed-Use Neighborhood (MUN) and Regional Commercial (RC) to Specific Plan SP within the Specific Plan Boundary and revise the Mobility Element Figure 4.3 “Circulation Plan” to remove Clinton Street south of Avenue 42.
- Withdraw the western 80-acre portion of the site from the Gateway Conceptual Specific Plan.
- Rescind the Thymes Square Project Master Plan and the Clinton Street Business Park Project Master Plan.
- Specific Plan – The Oasis at Indio Specific Plan is a regulatory document that establishes the zoning, land use designations, development standards, and design guidelines for the entire Specific Plan Area. The Specific Plan will implement the City's General Plan. The Specific Plan will be considered by the Planning Commission and City Council and adopted by Ordinance. Subsequent tract/parcel maps or site development plans must be in substantial compliance with the adopted Specific Plan. A zone map change from Specific Plan (MU-SP), Mixed-Use Neighborhood (MUN), and Regional Commercial (RC) to Specific Plan SP is required as part of the Specific Plan adoption.
- Subdivision Map – A tentative tract or parcel map is a basic tool for the implementation of a specific plan. The tentative map or parcel map will create the individual lots and associated planning areas. A Tentative Parcel Map has been prepared and will be considered by the City concurrently with the review of this Specific Plan.
- Development Agreement

## 5.1.6 Administrative Adjustments and Specific Plan Amendments

It is anticipated that certain modifications or adjustments to the Specific Plan text, exhibits, and/or project may be necessary during the development of the project. Amendments may be requested at any time pursuant to Section 65453(a) of the Government Code. Depending upon the nature of the request, environmental analysis may be required, pursuant to the CEQA Guidelines.

### 5.1.6.1 Administrative Adjustments

Administrative Adjustments are minor adjustments or modifications, as requested by an applicant, from the plans and standards of the Specific Plan that are consistent with the vision of The Oasis at Indio Specific Plan, are in substantial conformance with the intent of the Specific Plan provisions, and do not result in additional potentially significant environmental impacts as defined by CEQA. Minor modifications may be warranted to accommodate changes resulting from final design and engineering that cause adjustments in internal driveway alignments, location of utilities or other infrastructure, development of innovative product design, distribution of permitted uses within the Specific Plan, development of builder-level design guidelines, density transfers or other similar modifications deemed to be minor. The following may be considered for Administrative Adjustments:

- Inclusion of land uses not specified in Chapter 3, Development Regulations, but similar in intent and character subject to interpretation by the Community Development Director as specified in Section 5.1.4 Interpretation above.
- Determination of equivalent Development Standards.
- Revisions to exhibits, graphics, and text for purposes of clarification that do not substantially change the intent of The Oasis at Indio Specific Plan.
- Modifications necessary to comply with final Conditions of Approval or mitigation measures when adopted under subsequent actions.
- Modifications to performance standards included in the Specific Plan provided any such modifications provide substantially equivalent protection as the original standard.
- Modification of the acreage covered by a given Planning Area.
- Minor adjustments of any planning area boundary to implement a development plan (including lot line adjustments).
- A decrease in development intensity/density (non-residential square footage, lodging rooms, and/or residential units).
- Modifications to the conceptual phasing program, provided that public infrastructure improvements and services necessary to serve the development are available or will be provided concurrently with the development.
- Modifications to the conceptual design guidelines contained within this Specific Plan, which are intended to be flexible in their implementation.
- Adjustments to the alignment, location and sizing of utilities and facilities or a change in utility and/or public service provider may be approved by the City's Public Works Department, so long as the adjustments or changes are found to be in compliance with applicable plans and standards of the agency responsible for such utilities and facilities and do not result in significant environmental impacts.

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- Modifications to the design of the roadway cross-sections, alignment, width, and location of vehicle ingress and egress, and roadway alignment provided that the streets and drives have adequate capacity to handle the anticipated volumes of traffic and the design changes are supported by the City's Traffic Engineer.
- Any modification deemed by the Community Development Director to be minor modifications that are in keeping with the intent of Oasis at Indio Specific Plan and are in conformance with the City's General Plan.
- Minor adjustments to any of the development standards or regulations such as modification of wall heights for noise attenuation purposes, modification of allowable encroachments into setbacks, etc. that are specifically allowed under the Development Regulations of this Specific Plan.
- Approval of changes in construction materials and methods that are functionally equivalent or superior.
- Modification of any design element in this Specific Plan that improves circulation, reduces grading, improves drainage, improves infrastructure, or provides similar utility and reduces operations and maintenance costs or improves the level of sustainability.
- Determinations regarding issues, conditions, or situations that arise and are not addressed by this Specific Plan.

The Administrative Adjustments listed above are not comprehensive. Any adjustment that is deemed by the Community Development Director to be in substantial conformance with the purpose and intent of the Specific Plan shall be permitted, provided that the adjustment does not result in additional potentially significant environmental impacts as defined by CEQA.

The documentation of substantial conformance may include text and/or maps which describe the nature of all proposed modifications or adjustments to the Specific Plan. This application of substantial conformance with the adopted Specific Plan shall undergo any necessary technical review by City agencies as the Community Development Director or designee deems necessary.

The Community Development Director shall record the determination in writing with findings on which the determination is based. The Administrative Adjustment may be approved, with or without conditions, after making all the following findings:

1. The modifications are consistent with the goals and intent of the Specific Plan;
2. The physical characteristics of the site have been adequately assessed, and proposed building sites are of adequate size and shape to accommodate proposed uses and all other features of development;
3. There is supporting infrastructure, existing or available, consistent with the requirements of the Specific Plan, to accommodate the development without significantly lowering service levels; and
4. The development resulting from the approval of the Administrative Adjustment would not be detrimental to the public health, interest, safety, or general welfare and would not be detrimental or injurious to property or improvements in the vicinity and in the same Land Use District.

### Public Notice

No public review or public notice shall be required. The decision by the Community Development Director shall be final unless a call for review is made pursuant to Section 6.03.12 of the Unified Development Code.

#### 5.1.6.2 Specific Plan Amendments

Substantial amendments to the Specific Plan are modifications or amendments that change the intent, provisions, or development standards of The Oasis at Indio Specific Plan. Amendments shall constitute the following:

- Amendments to the Specific Plan that would result in an amendment to the City's adopted General Plan.
- Modifications to the Specific Plan boundaries to include properties not included in the Specific Plan at the time of approval (changes to planning area boundaries within the Specific Plan boundaries are deemed minor as noted above and would not require an amendment).
- Any changes in land use or development intensity that would result in substantial changes or potentially significant impacts not considered by the EIR.
- Any changes in land use development intensity that would increase the maximum development capacity as described in Chapter 2 of the Specific Plan and implemented by the Land Use Equivalency Table (Table 5-2).

Specific Plan Amendments shall be reviewed and processed pursuant to Section 6.05.03 of the Unified Development Code. The Specific Plan may be amended as often as deemed necessary by the Council in compliance with State law (Government Code Section 65453).

## 5.2 Implementation

Approval of The Oasis at Indio Specific Plan indicates acceptance by the City Council of a general framework for development within The Oasis at Indio boundaries. Part of that framework establishes specific development standards that constitute the zoning regulations for The Oasis at Indio Specific Plan. The implementation process described herein provides the mechanisms for the review and approval of subsequent development projects within The Oasis at Indio.

The intent of Section 5.2 is to streamline subsequent project approvals. Figure 5-1, *Permit Process*, and Table 5-1, *Review Authority* outline the procedures for the processing of implementing projects within the Specific Plan.

### 5.2.1 Planning Review - Administrative

Administrative planning review applies to the following proposed new or modified uses, activity, and/or structures allowed as a matter of right ("P" Permitted) or through an Administrative Use Permit ("AUP") per Table 3-1: Land Use Matrix, so long as the use, activity, and/or structure conform to all applicable

## Chapter 5 Administration and Implementation

development regulations and standards in Chapter 3 and meets the intent of the design guidelines in Chapter 4 of the Specific Plan.

### *Applicability*

Administrative Planning review is required for all projects that require a permit for new construction, reconstruction, rehabilitation, alteration, or other improvements to the exterior of a structure or site including:

- Residential development projects and residential mixed-use projects not subject to the Housing Crisis Act/SB 330
- Commercial development projects
- Industrial development projects

Exemptions:

- Additions, construction, reconstruction, alterations, improvements, and landscaping for a project developed in compliance with a previous planning review approval, and
- Replacement of exterior materials, including openings, with the same materials.

### *Review Authority*

The Director shall be the review authority for administrative planning reviews. The Director may, at his/her discretion, refer any administrative application to the Planning Commission for a decision rather than acting on it himself/herself.

### *Application Review and Approval Process*

The Director will review the project against all applicable use, development standards, and design guidelines. If the Director determines that the proposed use or building is allowed as a matter of right by The Oasis at Indio Specific Plan, conforms to all the applicable development and use standards, and meets the intent of the design standards and/or guidelines, the Director shall issue an approval. Staff will draft a Findings and Notice of Decision document containing all relevant findings and conditions of approval.

In order to track implementation of the project, as planning applications for Development Project Review are submitted, a tracking mechanism will be employed, using a form provided in Appendix C of this Specific Plan.

#### 5.2.1.1 Minor Modifications of Approved Plans

No change in the use or structure for which a permit or other approval has been issued is permitted unless the permit is modified as provided for in this Specific Plan. For the purposes of this Section, the modification of a permit may include modification of a planning review approval.

### *Applicability*

The Director may approve minor changes or modifications to approved plans provided that they meet the following:

1. They have no potential for significant offsite impacts and would not intensify any potentially detrimental effects of the project.
2. They are consistent with the original findings and conditions approved by the hearing body.
3. They do not change the use designated in the original use permit.
4. They are consistent with the development and design standards they were approved under.
5. They do not increase, reduce, or alter the size or shape of the premises beyond the following:
  - a. A five percent increase in building height;
  - b. A 10 percent increase in floor area or building coverage when calculated on a total, aggregate project basis; or
  - c. A 10 percent decrease in common open space.

#### *Public Notice*

No public review or public notice shall be required. The decision by the Community Development Director shall be final unless a call for review is made pursuant to Section 6.03.12 of the Unified Development Code.

#### **5.2.1.2 Administrative Variance**

Administrative Variances are minor deviations that are small in size from otherwise applicable standards and are unlikely to have any adverse effects on nearby properties or neighborhoods.

#### *Applicability*

Variances may be granted to vary or modify dimensional and performance standards, but Variances may not be granted to allow uses or activities that this Specific Plan does not authorize.

#### *Review Authority and Approval Process*

The Director may grant an Administrative Variance for adjustments from the development standards in Chapter 3 of the Specific Plan where, in his/her opinion, the intent of the ordinance can be achieved and equal performance obtained by granting an Administrative Variance. The authority to grant such variances shall be limited to requests that do not exceed a 20 percent deviation in the development standards for the following:

- a. Lot area
- b. Setbacks
- c. Parking regulations
- d. Encroachments into setbacks.

#### *Required Findings for Administrative Variances*

Administrative Variance shall include data or other evidence showing that the requested Administrative Variance conforms to the required findings set forth below. The Director may approve or approve with conditions an Administrative Variance application if all of the following findings are made.

1. There are exceptional or extraordinary circumstances or conditions applicable to the property involved that do not apply generally to property in the vicinity and identical zone, and that the granting of an Administrative Variance will not constitute a granting of a special privilege inconsistent with the limitations on the property in the vicinity and identical zone;

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2. The granting of the application is necessary to prevent a physical hardship which is not of the applicant's own actions or the actions of a predecessor in interest;
3. The granting of the application will not be detrimental or injurious to property or improvements in the vicinity, and will not be detrimental to the public health, safety, general welfare or convenience; and
4. The granting of the Administrative Variance will be consistent with the general purposes and objectives of this Specific Plan and of the General Plan.

### *Public Notice*

No public review or public notice shall be required. The decision by the Community Development Director shall be final unless a call for review is made pursuant to Section 6.03.12 of the Unified Development Code.

### 5.2.1.3 Administrative Use Permit

Uses specified as administratively permitted uses (AUP) within Chapter 3 "Development Regulations" of this Specific Plan shall be reviewed and approved by the City pursuant to the requirements of the Unified Development Code, Section 6.04.04 (Use Permits).

### *Public Notice*

No public review or public notice shall be required. The decision by the Community Development Director shall be final unless a call for review is made pursuant to Section 6.03.12 of the Unified Development Code.

### 5.2.1.4 Temporary Use Permit

Uses specified as temporary (TUP) within Chapter 3, "Development Regulations", of this Specific Plan intended to be of a limited duration of time and not permanently alter the character or physical facilities of the site where they occur shall be reviewed and approved by the City pursuant to the requirements of the Unified Development Code, Section 6.04.05 (Temporary Use Permits).

## 5.2.2 Planning Review – Ministerial

### *Applicability*

Ministerial planning review applies to the following types of proposed uses or buildings that qualify for streamlined ministerial planning review under California Government Code Sections 65650 et seq. ("State Supportive Housing Law"), 65660 et seq. ("State Low Barrier Navigation Centers Law"), and 65913.4 ("State Streamlined Ministerial Approval Process").

- Affordable housing developments (per SB 35).
- Residential developments of two or more units and mixed-use developments in which two-thirds of the project's square footage is used for residential purposes (per Housing Crisis Act/SB 330).
- Transitional or supportive housing (per SB 744/AB 2162).
- Low Barrier Navigation Centers (per SB 48).

*Review Authority and Approval Process*

The Planning Commission shall have the authority to conduct ministerial planning reviews to make sure that a project complies with all applicable objective plans, regulations, policies, development and design standards. Application review and procedure shall be processed in compliance with Section 6.04.03.E (Permits and Approvals) of the IUDC.

### 5.2.3 Planning Review – Discretionary

*Applicability*

Discretionary planning review applies to all projects requiring Planning Commission approval (such as Conditional Use Permits and Variances) and all projects that do not qualify for an administrative or ministerial planning review.

*Review Authority and Approval Process*

Discretionary Planning Review and procedures shall be processed in compliance with Section 6.04.03.F (Permits and Approvals) of the IUDC.

#### 5.2.3.1 Major Modifications of Approved Plans

No change in the use or structure for which a permit or other approval has been issued is permitted unless the permit is modified as provided for in this Specific Plan. For the purposes of this Section, the modification of a permit may include modification of a planning review approval.

*Applicability*

The Planning Commission may approve major changes or modifications to approved plans provided that they meet the following. This is an action item only, not subject to public hearing.

1. They have no potential for significant offsite impacts and would not intensify any potentially detrimental effects of the project.
2. They are consistent with the original findings and conditions approved by the hearing body.
3. They do not change the use designated in the original use permit.
4. They are consistent with the development and design standards they were approved under.
5. They do not increase, reduce, or alter the size or shape of the premises beyond the following:
  - a. A 10 percent increase in building height;
  - b. A 15 percent increase in floor area or building coverage when calculated on a total, aggregate project basis; or
  - c. A 15 percent decrease in common open space.

New Application. A request for changes in conditions of approval of a discretionary permit or a change in an approved site plan or building plan beyond those allowed under the thresholds for major modifications above shall be treated as a new application.

#### 5.2.3.2 Variance

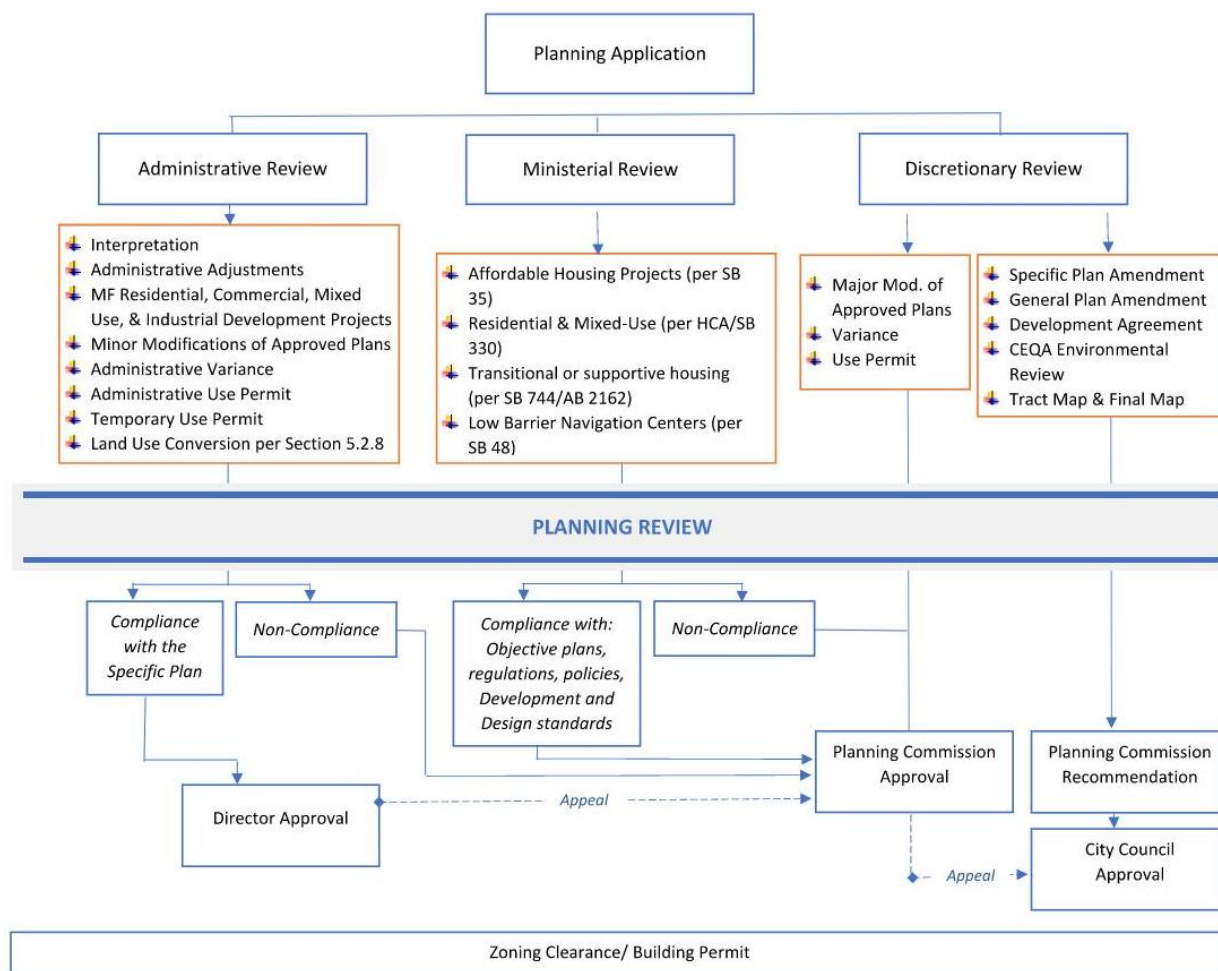
All Variances that do not qualify as an Administrative Variance are subject to discretionary review and requirements pursuant to Section 6.04.06.E (Variances) of the IUDC.

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### 5.2.3.3 Use Permits

Uses specified as conditionally permitted uses (CUP) within Chapter 3 “Development Regulations” of this Specific Plan shall be reviewed and approved by the City pursuant to the requirements of the Unified Development Code, Section 6.04.04 (Use Permits).

**Figure 5-1: Permit Process**



*Note: When a proposed project requires more than one permit application with more than one Review Authority, all project permits shall be processed concurrently as interrelated permits for a project and shall not be bifurcated. The highest designated Review Authority for all requested permits shall take final action on all permits.*

### 5.2.4 Pre-Application Review and Conferences

A Pre-Application Review is a review process and opportunity to:

1. Acquaint a potential application with information on relevant policies, code regulations, and other relevant criteria and procedures. This review is intended for large or complex projects and projects that are potentially controversial.

2. Provide a potential applicant with an early opportunity to meet with the City's Pre-Application Committee to identify possible major issues and discuss the City's review processes to help expedite permit processing and approvals.
3. To acquaint City staff with a potential applicant and application.

Pre-application reviews for subsequent development projects requiring an administrative review or discretionary planning review is not required.

## 5.2.5 Concurrent Processing

Concurrent Processing. When a proposed project requires more than one permit application with more than one Review Authority, all project permits shall be processed concurrently as interrelated permits for a project and shall not be bifurcated. The highest designated Review Authority for all requested permits shall take final action on all permits.

## 5.2.6 Notice of Decision; Appeals; Expiration, Extensions, and Modifications.

When making a decision to approve, approve with conditions, modify, or deny any discretionary permit, the responsible authority shall issue a Notice of Decision and make findings of fact as required.

### 5.2.6.1 Notice of Decision

Where Planning Review is conducted **by the Director**, a Notice of Decision shall be provided pursuant to Section 6.03.10 (Timing and Notice of Decision and Findings Required) of the IUDC. The Director shall provide a report at every regularly scheduled Planning Commission meeting of administrative decisions made by the Director or his/her designee.

Where Planning Review is conducted **by the Planning Commission**, a Notice of Decision of Planning Review shall be provided concurrently with the associated project approval (such as the Conditional Use Permit or Variance). See Section 6.03.10 (Timing and Notice of Decision and Findings Required) of the IUDC.

### 5.2.6.2 Appeals

Planning Review decisions are subject to the appeal provisions of Section 6.03.13 (Appeals).

### 5.2.6.3 Expiration, Extensions, and Modifications

Planning Review approval is effective and may only be extended or modified as provided for in Section 6.03.15 (Expiration and Extension) and Section 6.03.16 (Modification of Approved Plans) of the IUDC.

Table 5-1 Review Authority		
Review Authority	Request	Review Type
Community Development Director or Designee	Interpretations per Specific Plan Section 5.1.4	Administrative
	Administrative Adjustments per Specific Plan Section 5.1.6.1	
	Land Use Conversions per Specific Plan Section 5.2.7	
	Sign Permits and updated Master Sign Program	
	Zoning Clearance Letter	
	Administrative Planning Review per Specific Plan Section 5.2.1	
	Administrative Use Permit per Specific Plan Section 5.2.1.3	
	Administrative Variance per Specific Plan Section 5.2.1.2	
	Home Occupation Permit	
	Temporary Use Permit	
Planning Commission	Discretionary Planning Review per Specific Plan Section 5.2.3	Discretionary
	Conditional Use Permit per Specific Plan Section 5.2.3.3	
	Variance per Specific Plan Section 5.2.3.2	
	Certification of Public Convenience (ABC license)	
	Tentative Parcel Map	
	Major Modifications to an Approved Permit per Specific Plan Section 5.2.3.1	
	Appeal of Community Development Director’s Decision	
	Call for review a decision of the Director pursuant to IUDC Section 6.03.11	
City Council	Specific Plans and Amendments	Discretionary
	Tentative Tract Maps, Final Maps, and Improvements per Indio Municipal Code	

Table 5-1 Review Authority		
Review Authority	Request	Review Type
preceding Planning Commission recommendation	Development Agreements and Amendments per IUDC Section 6.05.02	
	General Plan Amendment per IUDC Chapter 6.06	
	Appeal of Planning Commission Decision pursuant to IUDC Section 6.03.12	
	Call for review a decision of the Director or Planning Commission pursuant to IUDC Section 6.03.11	

### 5.2.7 Land Use Conversion

The amount and type of land uses within the Specific Plan Area is programmatic and is intended to be flexible, responding to market demand and opportunities for unique or in-demand land uses. The mix of uses outlined in the Land Use section of this document is based upon anticipated demand for these uses at the time of writing.

Maximum intensities of land uses may be converted at any time by the project developer, based on the conversion factors outlined in Table 5-2, *Land Use Equivalency Conversion*. These conversion factors are based on trip generation, which allows a conversion program while keeping traffic generation stable. Implementation of a land use conversion will be reviewed by the Community Development Director as part of the related application and allowed as part of an administrative Planning review subject to substantial conformance with relevant development standards.

Scenario #2 reflects an illustrative alternative development scenario that is consistent with both (1) the maximums under Table 2-2 and (2) the land use equivalency conversion ratios in Section 5.2.3.

Table 5-2 Scenario #2				
Land Use District	PA	Acreage (Approximately)	Land Uses	
			Multi-Family Residential	Commercial Non-Residential Square Footage
Mixed-Use	1	61.9	20 DU/AC	71,600 square feet 128 Key Hotel/Motel
Mixed-Use	2	4.2		
Mixed-Use	3	9.3		
Industrial	4	92.8	N/A	1,806,290 square feet
Public ROW Dedication		2.0	N/A	

Table 5-2 Scenario #2				
Land Use District	PA	Acreage (Approximately)	Land Uses	
			Multi-Family Residential	Commercial Non-Residential Square Footage
I.I.D Substation		2.4	N/A	
Interior ROW		10.5	N/A	
<b>TOTALS</b>		<b>182.8</b>	<b>1,237 DU</b>	<b>1,877,890 and 128 Key Hotel/Motel</b>

Table 5-2 allows for Floor Area reallocations between the Land Use Categories utilizing conversion factors in Table 5-3. For example: 100 multi-family residential dwelling units is equivalent to 12.378 ksf or 12,378 square feet of Commercial/Retail Option A use. If 12,378 square feet were constructed using the conversion factors, that would eliminate the potential for 100 dwelling units (as counted against the 3,240 dwelling unit maximum). Because the conversion factors reflect equivalent trip generation, the overall trip generation associated with the reduction in 100 dwelling units and commensurate increase in 12,378 square feet of commercial/retail uses are considered “equivalent.”

**Table 5-3  
Land use Equivalency Conversion**

	Land Use (Unit)	Equivalency Ratios to Convert to these Land Use Types					
		MF Residential (DU) [a]	Commercial/Retail Option A (KSF) [b]	Commercial/Retail Option B (KSF) [c]	Hotel (Rooms)	Industrial (KSF) [d]	Office (KSF) [e]
From these Land Use Types:	MF Residential (DU) [a]	--	0.124	0.071	0.844	1.047	0.622
	Commercial/Retail Option A (KSF) [b]	8.079	--	--	6.815	8.455	5.023
	Commercial/Retail Option B (KSF) [c]	14.019	--	--	11.826	14.672	8.717
	Hotel (Rooms)	1.185	0.147	0.085	--	1.241	0.737
	Industrial (KSF) [d]	0.956	0.118	0.068	0.806	--	0.594
	Office (KSF) [e]	1.608	0.199	0.115	1.357	1.683	--
Land Use (Unit)	Quantity to Convert From	Land Use Equivalence					
		MF Residential (DU) [a]	Commercial/Retail Option A (KSF) [b]	Commercial/Retail Option B (KSF) [c]	Hotel (Rooms)	Industrial (KSF) [d]	Office (KSF) [e]
MF Residential (DU) [a]	100	--	12.378	7.133	84.355	104.658	62.177
Commercial/Retail Option A (KSF) [b]	100	807.864	--	--	681.477	845.497	502.306
Commercial/Retail Option B (KSF) [c]	100	1,401.929	--	--	1,182.60	1,467.24	871.661
Hotel (Rooms)	100	118.546	14.674	8.456	--	124.068	73.709
Industrial (KSF) [d]	100	95.549	11.827	6.816	80.601	--	59.410
Office (KSF) [e]	100	160.831	19.908	11.472	135.670	168.323	--

Notes

- [a] ITE LU 220: Multifamily Housing Low Rise Not Close to Rail Transit
- [b] ITE LU 822: Strip Retail Plaza less than 40K
- [c] ITE LU 821: Shopping Plaza 40K- 150K with Supermarket
- [d] ITE LU 155: High Cube Fulfillment Center Warehouse Sort
- [e] ITE LU 710: General Office Building

### 5.2.8 Conceptual Phasing

A diverse typology of residential and non-residential land uses is anticipated to achieve the Specific Plan vision. The extension of “A” Street (east-west internal roadway) connects the different land uses and the Specific Plan Area as a whole with the existing surrounding neighborhood and the regional circulation network. Buildout of the Specific Plan will take place in multiple phases based on market demand. The only factor limiting the phases is that infrastructure is available for the construction of that phase.

The Oasis at Indio will be phased to:

- Provide for the orderly build-out of the community based upon market demand.
- Provide adequate infrastructure to service the development.
- Phases may occur concurrently so long as the associated infrastructure is provided.

### 5.2.9 Maintenance and Ownership

The public and private improvements constructed within The Oasis at Indio shall be maintained through a combination of public and private entities as described in Table 5-3, Financing, Ownership, and Maintenance Responsibilities.

Maintenance will be funded by a combination of private and public funds. For common areas located within a planning area, the CC&Rs shall govern the maintenance responsibilities. The maintenance association(s) or property owners shall be responsible for private driveways, parking, open space areas, signage, landscaping, irrigation, common areas, on-site sewers, storm drains, BMPs, and other responsibilities as necessary. For areas outside the planning area boundaries, municipal maintenance districts may fund the maintenance of these areas. See Section 5.2.11 for possible financing mechanisms.

Table 5-4 Financing, Ownership, and Maintenance			
Facility	Financing	Ownership	Maintenance
<b>Public Street right-of-way (ROW)</b>			
• Curb to Curb Improvements	Developer	City	City
• Storm Water Drainage/Water Quality Facilities	Developer	City	City
• Water Infrastructure	Developer	IWA	IWA
• Sewer Infrastructure	Developer	VSD	VSD
• Dry Utilities ○ Electric ○ Natural Gas	Developer	IID SCG	IID SCG
• Landscaping including medians & parkways	Developer	City	City
• Sidewalks	Developer	City	City
• Streetlights	Developer	City	City
• Traffic control signal	Developer	City	City
• Traffic control signs	Developer	City	City
<b>"A" Street</b>			
• Curb to Curb Improvements	Developer	Private	Private/POA
• Water Infrastructure	Developer	IWA	IWA
• Sewer Infrastructure	Developer	VSD	VSD
• Dry Utilities ○ Electric ○ Natural Gas	Developer	IID SCG	IID SCG

<b>Table 5-4 Financing, Ownership, and Maintenance</b>			
<b>Facility</b>	<b>Financing</b>	<b>Ownership</b>	<b>Maintenance</b>
• Landscaping including parkways	Developer	Private	Private/POA
• Sidewalks	Developer	Private	Private/POA
• Traffic control signs	Developer	Private	Private/POA
• Streetlights	Developer	Private	Private/POA
<b>Internal Planning Areas</b>			
• On-Parcel Water Infrastructure	Developer	Private	Private/POA
• On-Parcel Sewer Infrastructure	Developer	Private	Private/POA
• On-Parcel Fire Line Infrastructure	Developer	Private	Private/POA
• Common Open Space	Developer	Private	Private/POA
• Common landscape areas	Developer	Private	Private/POA
• Fences & walls	Developer	Private	Private/POA
• Drive aisles and parking	Developer	Private	Private/POA
• Storm Water Drainage/Water Quality Facilities	Developer	Private	Private/POA
• Dry Utility Infrastructure	Developer	Private	Private/POA
• IID Substation	Developer	IID	IID

### 5.2.10 Development Agreement

The property owner and the City of Indio may enter into a development agreement on a single planning area or collection of planning areas therein. The development agreement may include, but not be limited to, methods for financing, acquisition, and construction of infrastructure, and the provision of economic opportunities for the City of Indio and employment and/or housing opportunities for community residents.

### 5.2.11 Financing for Construction, Operations and Maintenance

A facilities financing program is critical to this Specific Plan's implementation. The financing program ensures timely financing of public streets, utilities, and other necessary capital improvements.

Various options are available for the financing of capital improvements associated with Specific Plan development and for funding the future operations and maintenance costs of public improvements and facilities. The exact financing method will be determined in conjunction with the phasing of the improvements and facilities. Some of the possible funding mechanisms are listed in Table 5-4.

Table 5-5 Financing Mechanisms

Type	Description	Use
<p><b>Assessment Districts</b>  <b>"Assessment" includes, but is not limited to, "Special Assessment," "Benefit Assessment," and "Maintenance Assessment."</b></p>	<p>An Assessment District is a financing mechanism under the California Streets and Highway Code, Divisions 10 and 12, enabling the City, or special districts organized for the purpose of aiding in the development or improvement to, or within the district, to designate specific areas as an assessment district with the approval of a majority of the landowners based on financial obligations, and allows these Districts to issue bonds and collect special assessments to finance the improvements constructed or acquired by the District. Assessment District help each property owner pay a fair share of the costs of such improvements over a period of years at reasonable interest rates and ensures that the cost will be spread to all properties that receive direct and special benefit by the improvements constructed. A lien is placed against each property within the district, which is amortized over the life of the assessment, and is collected with regular property taxes.</p>	<p>Construct, and sometimes operate and maintain, public capital improvements, such as streets, sidewalks, curbs and gutters, water, sewer, gas, electric, lighting, drainage, and flood control, as well as landscaping and parking facilities.</p>
<p><b>Landscape and Lighting Districts</b></p>	<p>The Landscape and Lighting District provides and ensures the continued maintenance, servicing, administration and operation of various landscaped and lighting improvements and associated appurtenances located within the public right-of-way and dedicated landscape easements in various tracts throughout each district. Funding is determined annually, based upon the landscape, maintenance</p>	<p>Maintain certain improvements such as perimeter landscape, medians and center islands, planting materials, irrigation systems, drainage systems, detention basins, open space areas, public pedestrian paths, and entry monuments.</p>

Table 5-5 Financing Mechanisms

Type	Description	Use
	<p>and appurtenant facilities associated with the district, and costs associated with their maintenance. Costs are apportioned to the parcels within each district based on an equity-based formula.</p> <p>Annual assessments are established pursuant to the Landscape and Lighting Act of 1972, Part 2 of Division 15 of the California Streets and Highways Code.</p>	
<b>Property and Business Improvement District</b>	<p>A Property and Business improvement District is a form of a Benefit Assessment District. It is a geographically defined business area in which private property owners' group to gain legal standing and generate revenue to improve the services and facilities of the area.</p>	<p>Physical improvements and programs, including street furnishings, signs, lighting, street and sidewalk cleaning, retail retention and recruitment, and marketing for businesses.</p>
<b>Cost Reimbursement District</b>	<p>A Cost Reimbursement District is a form of a Benefit Assessment District that allows a developer to recoup costs of initial infrastructure with capacity to serve areas beyond the boundaries of the development. It requires the initial installation of the improvement by a developer who then receives cost reimbursement from future developers as they utilize the improvements.</p>	<p>Construct major infrastructure improvements.</p>
<b>Mello-Roos District/Community Facility Districts</b>	<p>The Mello-Roos Community Facilities Act of 1982 (Government Code §53321) is strictly a funding mechanism that enables the County, special districts, and school districts to establish Community Facility Districts to levy special taxes to fund facilities and services within the district.</p>	<p>Develop schools, transportation facilities, libraries, recreation, and infrastructure. Maintenance of public facilities and services, such as parks, open space, storm drain, fire and police.</p>

Table 5-5 Financing Mechanisms

Type	Description	Use
<p><b>Special District/Community Services District</b></p>	<p>A special district is a separate entity that provides facilities and services in a defined area (Government Code §16271). Special Districts may serve a single function (e.g., vector control) or multiple functions (e.g., County Service Area), in which case they are often referred to as Community Services Districts (Government Code §61000, et seq). An Enterprise Special District charges fees for customer services, while a Non-Enterprise Special District does not charge user fees. A Non-Enterprise Special District provides services that benefit the entire community, not just specific individuals. An Independent Special District elects a Board of Directors, either by registered voters, or by landowners. A Dependent Special District is governed by an existing legislative body, such as the County Board of Supervisors.</p>	<p>Construct facilities and provide public services. Commonly used for parks and recreation, water, sewer, fire protection.</p>
<p><b>Developer-Paid Impact Fees</b></p>	<p>Impact fees are direct charges to developers that are collected primarily upon approval of a building permit, certificate of occupancy, or final inspection by the County as a condition of development project approval. The purpose of the fee must directly relate to the need created by the development. The amount must be proportional to the cost of the improvement to mitigate the impact.</p>	<p>Construct transportation, infrastructure, parks and recreation facilities.</p>
<p><b>Conventional Subdivision Financing</b></p>	<p>This method is to finance with a lender the construction of necessary public facilities and infrastructure to serve the subdivision along with the development of residential</p>	<p>Any infrastructure or public facilities necessary to support the development of the subdivision land uses, including</p>

Table 5-5 Financing Mechanisms

Type	Description	Use
	and/or commercial uses in the subdivision.	roads, parks, water and storm water facilities.
<b>Land Reservation, Offers of Dedication, and/or Easements</b>	Land reservation, dedications and easements can be granted to the primary or subsequent developer of the Specific Plan by a public agency or entity to construct improvements for that agency or entity to manage, or the developer may give such grants to a public agency/entity to construct and manage facilities.	Construct and maintain utilities, rights-of-way, open space, parks and recreation, and other infrastructure.
<b>Public Utility District</b>	A Public Utility District is a form of a Special District that may be created pursuant to the State Public Utility District Act (Public Utilities Code §15501-18055). A Public Utility District can provide one or more services to a given geographical area. It is managed by an elected Board of Directors residing within the District.	Construct and maintain utilities, including energy.
<b>Reimbursement Agreement</b>	A Developer Reimbursement Agreement can take the form of a written contract between the primary developer in the Specific Plan Area and subsequent developers or property owners. The primary developer constructs infrastructure improvements for an overall area, and then is reimbursed or partially reimbursed the costs by subsequent developers or property owners benefitting from the improvement.	Construct streets and other transportation or parking facilities; water, sewer, storm water infrastructure.
<b>Property Owners Association</b>	A Property Owners' Association (POA) can include all types of residential units and sometimes businesses and commercial properties. POAs often have a Board of Directors elected by the property owners in the given area managed by the POA. Each property owner pays dues to the	Maintain landscaping, open space and other areas, as well as recreation facilities, owned in common by the POA property owners.

Table 5-5 Financing Mechanisms		
Type	Description	Use
	POA, which can then be used by the POA to manage landscaping, recreation, and other commonly owned facilities.	
<b>Transportation Management Association</b>	A Transportation Management Association (TDA) is a non-profit, member-controlled organization that provides transportation services in a geographic area. It is often a public-private partnership that provides Transportation Demand Management (TDM) measures that reduce vehicle miles travelled, encourage carpooling, encourage alternative transportation, and can also provide parking management. The Association is often more cost effective than a single business in offering TDM measures. A TDA can be established for the Specific Plan Area, with residents, employers, employees and customers in the area pooling resources to reduce transportation infrastructure needs and parking demand, thereby reducing transportation and parking infrastructure costs.	A TDA can be established for the Specific Plan Area, with residents, employers, employees, and customers in the area pooling resources to reduce transportation infrastructure needs and parking demand, thereby reducing infrastructure costs and the amount of land reserved for parking spaces.

Appendix A:  
Airport Land Use Commission  
(ALUC)





# RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

February 13, 2024

Kevin Snyder, Director of Community Development  
City of Indio, Planning Department  
100 Civic Center Mall,  
Indio, CA 92201

**CHAIR**  
Steve Manos  
Lake Elsinore

**VICE CHAIR**  
Russell Betts  
Desert Hot Springs

**COMMISSIONERS**

John Lyon  
Riverside

Steven Stewart  
Palm Springs

Richard Stewart  
Moreno Valley

Michael Geller  
Riverside

Vernon Poole  
Murrieta

**STAFF**

**Director**  
Paul Rull

Simon Houseman  
Barbara Santos  
Jackie Vega

County Administrative Center  
4080 Lemon St., 14<sup>th</sup> Floor.  
Riverside, CA 92501  
(951) 955-5132

[www.rcaluc.org](http://www.rcaluc.org)

**RE: AIRPORT LAND USE COMMISSION (ALUC) DEVELOPMENT REVIEW**

File No.: ZAP1093BD23  
Related File Nos.: GP-23-0001 (General Plan Amendment), SP-0001 (Specific Plan), ZMA-23-0001 (Zoning Map Amendment), TPM-23-0001 (Tentative Parcel Map)  
Compatibility Zone: Zones C, D, E  
APNs: 610-020-001, 610-020-010, 610-020-013, 610-020-021, 610-020-034, 610-020-036

Dear Mr. Snyder:

On February 8, 2024, the Riverside County Airport Land Use Commission (ALUC) found City of Indio Case Nos. GP-23-0001 (General Plan Amendment), SP-0001 (Specific Plan), ZMA-23-0001 (Zoning Map Amendment), TPM-23-0001 (Tentative Parcel Map), a proposal to establish the Oasis at Indio Specific Plan on approximately 186 acres which includes 66 acres of Mixed-Use in Planning Area 1; 5 acres of Mixed-Use in Planning Area 2, 10 acres of Mixed-Use in Planning Area 3, and 100 acres of Industrial in Planning Area 4, located southerly of Avenue 42, westerly of Monroe Street, and northerly of Interstate 10, and the applicant also proposes amending the site's General Plan land use designation from Mixed Use Neighborhood and Regional Commercial to Specific Plan, and also change the site's zoning from Specific Plan, Mixed Use Neighborhood, and Regional Commercial to Specific Plan, and the applicant also proposes to divide the site into 4 parcels (one for each Planning Area), **CONSISTENT** with the 2004 Bermuda Dunes Airport Land Use Compatibility Plan

Supporting documentation was provided to the Airport Land Use Commission and is available online at [www.rcaluc.org](http://www.rcaluc.org), click Agendas, click 02-08-2024 Agenda, Bookmark Agenda Item 3.1.

If you have any questions, please contact me at (951) 955-6893.

Sincerely,  
RIVERSIDE COUNTY AIRPORT LAND USE COMMISSION

Paul Rull, ALUC Director

Attachment: Notice of Airport in Vicinity

cc: B.H. Indio, LLC (applicant/property owner)  
MIG, Inc. (representative)  
Ann Goodwyn, Manager, Bermuda Dunes Executive Airport  
ALUC Case File

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# Appendix B: Track Implementation Form



# Appendix B – Project Tracking Form

## The Oasis at Indio Specific Plan

### Land Use Tracking (Prototype)

Date \_\_\_\_\_

Application File No. \_\_\_\_\_

Site Plan No. \_\_\_\_\_

Parcel No. \_\_\_\_\_

The Oasis at Indio Specific Plan identifies total development for residential units, commercial square footage, hotel rooms, and industrial warehouse square footage.

The Specific Plan allows for a baseline land use of:

- 3,240 dwelling units;
- 20,000 sf of non-residential uses; and
- 1,806,290 sf of industrial uses.

Complete the form for items applicable to the proposed project.

Use	Total	Parcel #
<b>Residential</b>		
Total baseline		
Previously allocated		
Proposed DU		
Remaining DU		
<b>Commercial</b>		
Total baseline		
Previously allocated		
Proposed SF		
Remaining SF		
<b>Hotel</b>		
Total baseline		
Previously allocated		
Proposed room/keys		
Remaining room/keys		
<b>Industrial</b>		
Total baseline		
Previously allocated		
Proposed SF		
Remaining SF		
<b>Office</b>		
Total baseline		
Previously allocated		
Proposed SF		
Remaining SF		